



Expansion of Hong Kong International Airport into a Three-Runway System

Construction Phase Quarterly EM&A Report
No.19 (1 July to 30 September 2020)

February 2021

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**This Construction Phase Quarterly EM&A Report No. 19 has been
reviewed and certified by**

the Environmental Team Leader (ETL) in accordance with

Section 15.4 of the Updated EM&A Manual

Certified by:

A handwritten signature in black ink, appearing to read 'Terence Kong', written in a cursive style.

Terence Kong
Environmental Team Leader (ETL)
Mott MacDonald Hong Kong Limited

Date

1 February 2021

Our Ref : 60440482/C/JCHL210201

By Email

Airport Authority Hong Kong
HKIA Tower, 1 Sky Plaza Road
Hong Kong International Airport
Lantau, Hong Kong

Attn: Mr. Lawrence Tsui, Principal Manager, Environmental Compliance

1 February 2021

Dear Sir,

Contract No. 3102
3RS Independent Environmental Checker Consultancy Services

Quarterly EM&A Report No. 19 (For 1 July 2020 to 30 September 2020)

Reference is made to the Environmental Team's submission of Quarterly EM&A Report No.19 (For 1 July 2020 to 30 September 2020) under section 15.4 of the Updated EM&A Manual certified by the ET Leader on 1 February 2021.

We would like to inform you that we have no adverse comment and verify the captioned submission.

Should you have any query, please feel free to contact the undersigned at 3922 9376.

Yours faithfully,
AECOM Asia Co. Ltd.



Jackel Law
Independent Environmental Checker

c.c. Mott MacDonald – Terence Kong (ETL)

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Abbreviations

3RS	Three-Runway System
AAHK	Airport Authority Hong Kong
AECOM	AECOM Asia Company Limited
AFCD	Agriculture, Fisheries and Conservation Department
AIS	Automatic Information System
ANI	Encounter Rate of Number of Dolphins
APM	Automated People Mover
AW	Airport West
BHS	Baggage Handling System
CAP	Contamination Assessment Plan
CAR	Contamination Assessment Report
CWD	Chinese White Dolphin
DCM	Deep Cement Mixing
DEZ	Dolphin Exclusion Zone
DO	Dissolved Oxygen
EAR	Ecological Acoustic Recorder
EIA	Environmental Impact Assessment
EM&A	Environmental Monitoring & Audit
EMIS	Environmental Mitigation Implementation Schedule
EP	Environmental Permit
EPD	Environmental Protection Department
ET	Environmental Team
FCZ	Fish Culture Zone
HKBCF	Hong Kong-Zhuhai-Macao Bridge Hong Kong Boundary Crossing Facilities
HKIA	Hong Kong International Airport
HSF	High Speed Ferry
IEC	Independent Environmental Checker
LKC	Lung Kwu Chau
MTCC	Marine Traffic Control Centre
MMHK	Mott MacDonald Hong Kong Limited
MMWP	Marine Mammal Watching Plan
MSS	Maritime Surveillance System
MTRMP-CAV	Updated Marine Travel Routes and Management Plan for Construction and Associated Vessel
NEL	Northeast Lantau
NWL	Northwest Lantau
PAM	Passive Acoustic Monitoring
SC	Sha Chau
SCZ	Speed Control Zone
SCLKMP	Sha Chau and Lung Kwu Chau Marine Park
SS	Suspended Solids
STG	Encounter Rate of Number of Dolphin Sightings
SWL	Southwest Lantau
The Manual	The Updated EM&A Manual

The Project	The Expansion of Hong Kong International Airport into a Three-Runway System
The SkyPier Plan	Marine Travel Routes and Management Plan for High Speed Ferries of SkyPier
TSP	Total Suspended Particulates
WL	West Lantau
WMP	Waste Management Plan

Executive Summary

The “Expansion of Hong Kong International Airport into a Three-Runway System” (the Project) serves to meet the future air traffic demands at Hong Kong International Airport (HKIA). On 7 November 2014, the Environmental Impact Assessment (EIA) Report (Register No.: AEIAR-185/2014) for the Project was approved and an Environmental Permit (EP) (Permit No.: EP-489/2014) was issued for the construction and operation of the Project.

Airport Authority Hong Kong (AAHK) commissioned Mott MacDonald Hong Kong Limited (MMHK) to undertake the role of Environmental Team (ET) for carrying out the Environmental Monitoring & Audit (EM&A) works during the construction phase of the Project in accordance with the Updated EM&A Manual (the Manual).

This is the 19th Construction Phase Quarterly EM&A Report for the Project which summarises the monitoring results and audit findings of the EM&A programme during the reporting period from 1 July 2020 to 30 September 2020.

Key Activities in the Reporting Period

The key activities of the Project carried out in the reporting period included reclamation works and land-based works. Works in the reclamation areas included deep cement mixing (DCM) works, marine filling, seawall and facilities construction, together with runway and associated works. Land-based works on existing airport island involved mainly airfield works, foundation and substructure work for Terminal 2 expansion, modification and tunnel work for Automated People Mover (APM) and Baggage Handling System (BHS), and preparation work for utilities, with activities include site establishment, site office construction, road and drainage works, cable ducting, demolition, piling, and excavation works.

EM&A Activities Conducted in the Reporting Period

The EM&A programme was undertaken in accordance with the Manual of the Project. Summary of the monitoring activities during this reporting period is presented as below:

Monitoring Activities	Number of Sessions
1-hour Total Suspended Particulates (TSP) air quality monitoring	96
Noise monitoring	52
Water quality monitoring	38
Vessel line-transect surveys for Chinese White Dolphin (CWD) monitoring	6
Land-based theodolite tracking survey effort for CWD monitoring	6

Environmental auditing works, including weekly site inspections of construction works conducted by the ET and bi-weekly site inspections conducted by the Independent Environmental Checker (IEC), audit of SkyPier High Speed Ferries (HSF), audit of construction and associated vessels, and audit of implementation of Marine Mammal Watching Plan (MMWP) and Dolphin Exclusion Zone (DEZ) Plan, were conducted in the reporting period. Based on information including ET’s observations, records of Maritime Surveillance System (MSS), and contractors’ site records, it is noted that environmental pollution control and mitigation measures were properly implemented and construction activities of the Project in the reporting period did not introduce adverse impacts to the sensitive receivers.

Snapshots of Good Environmental Practices in the Reporting Period

		
<p>Wasted lead acid batteries recycling via local battery recycler</p>	<p>Plastic waste recovery from the expired safety helmets</p>	<p>Dust suppression control management plan</p>

Key examples of good site practices implemented in the Project are highlighted as below:

1. Wasted lead acid batteries dismantled from APM trains were collected for recycling by a battery recycler to save the landfill space, eliminate the risk of pollution to the environment, and minimize the wastage.
2. Expired safety helmets were collected and delivered to a local recycler to facilitate the recovery of plastic.
3. Dust suppression control management plan for the new reclaimed land area was devised which consisted of the travelling of water browsers along the designated vehicle travelling routes, and establishment of water mist canon for the dust suppression during the handling of construction materials.

Summary Findings of the EM&A Programme

The monitoring works for construction dust, construction noise, water quality, construction waste, landscape & visual, and CWD were conducted during the reporting period in accordance with the Manual.

Monitoring results of construction dust, construction noise, construction waste, and CWD monitoring did not trigger the corresponding Action and Limit Levels in the reporting period.

The water quality monitoring results for all parameters, except dissolved oxygen (DO), obtained during the reporting period were within the corresponding Action and Limit Levels stipulated in the EM&A programme. Relevant investigation and follow-up actions will be conducted according to the EM&A programme if the corresponding Action and Limit Levels are triggered. For DO, some testing results triggered the relevant Action or Limit Level, and the corresponding investigations were conducted accordingly. The investigation findings concluded that the cases were not related to the Project. To conclude, the construction activities in the reporting period did not introduce adverse impact to all water quality sensitive receivers.

The key findings of the EM&A programme during the reporting period is summarised as below:

	Yes	No	Details	Analysis / Recommendation / Remedial Actions
Breach of Limit Level [^]		√	No breach of Limit Level was recorded.	Nil
Breach of Action Level [^]		√	No breach of Action Level was recorded.	Nil
Complaints Received	√		A complaint regarding suspected improper chemical waste disposal at the pier near Marina Garden, Tuen Mun was received on 6 Jul 2020.	ET requested the concerned Contractor for details on the complaint. Based on information provided by Contractor, no chemical waste was generated from the Contractor. The case was considered closed.
			A complaint regarding the discharge of muddy water from the construction site which was near Aviation Fuel Supply Company (AFSC) Operation Limited premise to the surrounding surface water channel was received on 13 Jul 2020.	ET investigated the related work contracts that carried out construction activities at or near the alleged area. Based on information provided by contractors and ET's inspection findings, no malpractices were observed. The case was considered closed.
			A complaint regarding dust issue at Chek Lap Kok South Road was received on 28 Aug 2020.	ET investigated the potential related work contracts whose barges moored at or near the alleged area. Based on information provided by contractors and ET's inspection findings, no malpractices were observed. The case was considered closed.
Notification of any summons and status of prosecutions		√	No notification of summons or prosecution were received.	Nil
Changes that affect the EM&A		√	There was no change to the construction works that may affect the EM&A.	Nil

Remarks:

[^]Only triggering of Action or Limit Level found related to Project works is counted as Breach of Action or Limit Level.

1 Introduction

1.1 Background

On 7 November 2014, the Environmental Impact Assessment (EIA) Report (Register No.: AEIAR-185/2014) for the “Expansion of Hong Kong International Airport into a Three-Runway System” (the Project) was approved and an Environmental Permit (EP) (Permit No.: EP-489/2014) was issued for the construction and operation of the Project.

Airport Authority Hong Kong (AAHK) commissioned Mott MacDonald Hong Kong Limited (MMHK) to undertake the role of Environmental Team (ET) for carrying out the Environmental Monitoring & Audit (EM&A) works during the construction phase of the Project in accordance with the Updated EM&A Manual (the Manual) submitted under EP Condition 3.1¹. AECOM Asia Company Limited (AECOM) was employed by AAHK as the Independent Environmental Checker (IEC) for the Project.

The Project covers the expansion of the existing airport into a three-runway system (3RS) with key project components comprising land formation of about 650 ha and all associated facilities and infrastructure including taxiways, aprons, aircraft stands, a passenger concourse, an expanded Terminal 2, all related airside and landside works and associated ancillary and supporting facilities. The submarine aviation fuel pipelines and submarine power cables also require diversion as part of the works.

Construction of the Project is to proceed in the general order of diversion of the submarine aviation fuel pipelines, diversion of the submarine power cables, land formation, and construction of infrastructure, followed by construction of superstructures.

The updated overall phasing programme of all construction works was presented in Appendix A of the Construction Phase Monthly EM&A Report No. 7 and the contract information was presented in Appendix A of the Construction Phase Monthly EM&A Report No. 54.

1.2 Scope of this Report

This is the 19th Construction Phase Quarterly EM&A Report for the Project which summarises the key findings of the EM&A programme during the reporting period from 1 July 2020 to 30 September 2020.

1.3 Project Organisation

The Project’s organisation structure is provided in **Appendix A**. Contact details of the key personnel have been updated and provided in **Table 1.1**.

¹ The Manual is available on the Project’s dedicated website (accessible at: <http://env.threerunwaysystem.com/en/index.html>)

Table 1.1: Contact Information of Key Personnel

Party	Position	Name	Telephone
Project Manager's Representative (Airport Authority Hong Kong)	Principal Manager, Environmental Compliance, Sustainability	Lawrence Tsui	2183 2734
Environmental Team (ET) (Mott MacDonald Hong Kong Limited)	Environmental Team Leader	Terence Kong	2828 5919
	Deputy Environmental Team Leader	Heidi Yu	2828 5704
	Deputy Environmental Team Leader	Daniel Sum	2585 8495
Independent Environmental Checker (IEC) (AECOM Asia Company Limited)	Independent Environmental Checker	Jackel Law	3922 9376
	Deputy Independent Environmental Checker	Roy Man	3922 9141

Reclamation Works:

Party	Position	Name	Telephone
Contract 3205 DCM (Package 5) (Bachy Soletanche - Sambo Joint Venture)	Deputy Project Director	Min Park	9683 0765
	Environmental Officer	Steven Chan	6288 0189
Contract 3206 Main Reclamation Works (ZHEC-CCCC-CDC Joint Venture)	Project Manager	Alan Mong	3763 1352
	Environmental Officer	Kwai Fung Wong	3763 1452

Airfield Works:

Party	Position	Name	Telephone
Contract 3301 North Runway Crossover Taxiway (FJT-CHEC-ZHEC Joint Venture)	Deputy Project Director	Kin Hang Chung	9800 0048
	Environmental Officer	Joe Wong	6182 0351
Contract 3302 Eastern Vehicular Tunnel Advance Works (China Road and Bridge Corporation)	Project Manager	Dickey Yau	5699 4503
	Environmental Officer	Dennis Ho	5645 0563
Contract 3303 Third Runway and Associated Works (SAPR Joint Venture)	Project Manager	Andrew Keung	6277 6628
	Environmental Officer	Max Chin	6447 5707
Contract 3307 Fire Training Facility (Paul Y. Construction Company Limited)	Project Manager	Steven Meredith	6109 1813
	Environmental Officer	Albert Chan	9700 1083

Third Runway Concourse:

Party	Position	Name	Telephone
Contract 3402 New Integrated Airport Centres Enabling Works (Wing Hing Construction Co., Ltd.)	Contract Manager	Michael Kan	9206 0550
	Environmental Officer	Lisa He	5374 3418
Contract 3403 New Integrated Airport Centres Building and Civil Works (Sun Fook Kong Construction Limited)	Project Manager	Alice Leung	9220 3162
	Environmental Officer	Alpha Chia	9626 1114
Contract 3405 Third Runway Concourse Foundation and Substructure Works (China Road and Bridge Corporation – Bachy Soletanche Group Limited – LT Sambo Co., Ltd. Joint Venture)	Project Manager	Francis Choi	9423 3469
	Environmental Officer	Jacky Lai	9028 8975

Terminal 2 (T2) Expansion:

Party	Position	Name	Telephone
Contract 3503 Terminal 2 Foundation and Substructure Works (Leighton – Chun Wo Joint Venture)	Project Manager	Eric Wu	3973 1718
	Environmental Officer	Malcolm Leung	3973 0850

Automated People Mover (APM) and Baggage Handling System (BHS):

Party	Position	Name	Telephone
Contract 3601 New Automated People Mover System (TRC Line) (CRRRC Puzhen Bombardier Transportation Systems Limited and CRRRC Nanjing Puzhen Co., Ltd. Joint Venture)	Project Manager	Hongdan Wei	158 6180 9450
	Environmental Officer	Jasmine Tso	5968 6926
Contract 3602 Existing APM System Modification Works (Niigata Transys Co., Ltd.)	Project Manager	Kunihiko Tatecho	9755 0351
	Environmental Officer	Yolanda Gao	5399 3509
Contract 3603 3RS Baggage Handling System (VISH Consortium)	Project Manager	K C Ho	9272 9626
	Environmental Officer	Eric Ha	9215 3432

Construction Support (Facilities):

Party	Position	Name	Telephone
Contract 3721 Construction Support Infrastructure Works (China State Construction Engineering (Hong Kong) Ltd.)	Site Agent	Thomas Lui	9011 5340
	Environmental Officer	Xavier Lam	9493 2944
Contract 3722 Western Support Area – Construction Support Facilities (Tapco Construction Company Limited and Konwo Modular House Limited Joint Venture)	Deputy Project Director	Philip Kong	9049 3161
	Environmental Officer	Sampson Lo	9752 9118

Airport Support Infrastructure:

Party	Position	Name	Telephone
Contract 3801 APM and BHS Tunnels on Existing Airport Island (China State Construction Engineering (Hong Kong) Ltd.)	Project Manager	Tony Wong	9642 8672
	Environmental Officer	Federick Wong	9842 2703
Contract 3802 APM and BHS Tunnels and Related Works (Gammon Construction Limited)	Project Director	John Adams	6111 6989
	Environmental Officer	Andy Leung	9489 0035

Construction Support (Services / Licences):

Party	Position	Name	Telephone
Contract 3901A Concrete Batching Facility (K. Wah Concrete Company Limited)	Project Manager	Benedict Wong	9553 2806
	Environmental Officer	C P Fung	9874 2872
Contract 3901B Concrete Batching Facility (Gammon Construction Limited)	Senior Project Manager	Gabriel Chan	2435 3260
	Environmental Officer	Rex Wong	2695 6319

1.4 Contact information for the Project

The contact information for the Project is provided in **Table 1.2**. The public can contact us through the following channels if they have any queries and comments on the environmental monitoring data and project related information.

Table 1.2: Contact Information of the Project

Channels	Contact Information
Hotline	3908 0354
Email	env@3rsproject.com
Fax	3747 6050
Postal Address	Airport Authority Hong Kong HKIA Tower 1 Sky Plaza Road Hong Kong International Airport Lantau Hong Kong Attn: Environmental Team Leader Mr Terence Kong c/o Mr Lawrence Tsui (TRD)

1.5 Summary of Construction Works

The key activities of the Project carried out in the reporting period included reclamation works and land-based works. Works in the reclamation areas included DCM works, marine filling, seawall and facilities construction, together with runway and associated works. Land-based works on existing airport island involved mainly airfield works, foundation and substructure work for Terminal 2 expansion, modification and tunnel work for APM and BHS systems, and preparation work for utilities, with activities include site establishment, site office construction, road and drainage works, cable ducting, demolition of existing facilities, piling, and excavation works.

The locations of the key construction activities are presented in **Figure 1.1**.

1.6 Summary of EM&A Programme Requirements

The status for all environmental aspects is presented in **Table 1.3**. The EM&A requirements remained unchanged during the reporting period.

Table 1.3: Summary of Status for All Environmental Aspects under the Updated EM&A Manual

Parameters	EM&A Requirements	Status
Air Quality		
Baseline Monitoring	At least 14 consecutive days before commencement of construction work	The baseline air quality monitoring result has been reported in Baseline Monitoring Report and submitted to EPD under EP Condition 3.4.
Impact Monitoring	At least 3 times every 6 days	On-going
Noise		
Baseline Monitoring	Daily for a period of at least two weeks prior to the commencement of construction works	The baseline noise monitoring result has been reported in Baseline Monitoring Report and submitted to EPD under EP Condition 3.4.
Impact Monitoring	Weekly	On-going
Water Quality		
General Baseline Water Quality Monitoring for reclamation, water jetting and field joint works	Three days per week, at mid-flood and mid-ebb tides, for at least four weeks prior to the commencement of marine works.	The baseline water quality monitoring result has been reported in Baseline Water Quality Monitoring Report and submitted to EPD under EP Condition 3.4.

Parameters	EM&A Requirements	Status
General Impact Water Quality Monitoring for reclamation, water jetting and field joint works	Three days per week, at mid-flood and mid-ebb tides.	On-going
Initial Intensive Deep Cement Mixing (DCM) Water Quality Monitoring	At least four weeks	The Initial Intensive DCM Monitoring Report was submitted and approved by EPD in accordance with the Detailed Plan on DCM.
Regular DCM Water Quality Monitoring	Three times per week until completion of DCM works.	On-going
Sewerage and Sewage Treatment		
Methodology for carrying out annual sewage flow monitoring for concerned gravity sewer	Methodology to be prepared and submitted to EPD at least one year before commencement of the operation of 3RS	To be prepared and submitted to EPD
Details of the routine H ₂ S monitoring system for the sewerage system of 3RS	Details to be prepared and submitted to EPD at least one year before commencement of the operation of 3RS	To be prepared and submitted to EPD
Waste Management		
Waste Monitoring	At least weekly	On-going
Land Contamination		
Supplementary Contamination Assessment Plan (CAP)	At least 3 months before commencement of any soil remediation works.	The Supplementary CAP was submitted and approved by EPD pursuant to EP condition 2.20.
Contamination Assessment Report (CAR)	CAR to be submitted for golf course	The CAR for Golf Course was submitted to EPD.
	CAR to be submitted for Terminal 2 Emergency Power Supply Systems	The CARs for Terminal 2 Emergency Power Supply System Nos. 1 (Volumes 1 and 2), 2, 3, and 5 were submitted to EPD.
Terrestrial Ecology		
Pre-construction Egret Survey Plan	Once per month in the breeding season between April and July, prior to the commencement of HDD drilling works.	The Egret Survey Plan was submitted and approved by EPD under EP Condition 2.14.
Ecological Monitoring	Monthly monitoring during the HDD construction works period from August to March.	The terrestrial ecological monitoring at Sheung Sha Chau was completed in January 2019.
Marine Ecology		
Pre-Construction Phase Coral Dive Survey	Prior to marine construction works	The Coral Translocation Plan was submitted and approved by EPD under EP Condition 2.12.
Coral Translocation	-	The coral translocation was completed on 5 January 2017.
Post-translocation Monitoring	As per an enhanced monitoring programme based on the Coral Translocation Plan	The post-translocation monitoring programme according to the Coral Translocation Plan was completed in April 2018.
Chinese White Dolphins (CWD)		
Baseline Monitoring	6 months of baseline surveys before the commencement of land formation related construction works.	Baseline CWD results were reported in the CWD Baseline Monitoring Report

Parameters	EM&A Requirements	Status
	Vessel line transect surveys: Two full surveys per month; Land-based theodolite tracking surveys: Two days per month at the Sha Chau station and two days per month at the Lung Kwu Chau station; and Passive Acoustic Monitoring (PAM): For the whole duration of baseline period.	and submitted to EPD in accordance with EP Condition 3.4.
Impact Monitoring	Vessel line transect surveys: Two full surveys per month; Land-based theodolite tracking surveys: One day per month at the Sha Chau station and one day per month at the Lung Kwu Chau station; and PAM: For the whole duration for land formation related construction works.	On-going
Landscape and Visual		
Landscape and Visual Plan	At least 3 months before the commencement of construction works on the formed land of the Project.	The Landscape & Visual Plan was submitted to EPD under EP Condition 2.18
Baseline Monitoring	One-off survey within the Project site boundary prior to commencement of any construction works	The baseline landscape & visual monitoring result has been reported in Baseline Monitoring Report and submitted to EPD under EP Condition 3.4.
Impact Monitoring	Weekly	On-going
Environmental Auditing		
Regular site inspection	Weekly	On-going
Marine Mammal Watching Plan (MMWP) implementation measures	Monitor and check	On-going
Dolphin Exclusion Zone (DEZ) Plan implementation measures	Monitor and check	On-going
SkyPier High Speed Ferries (HSF) implementation measures	Monitor and check	On-going
Construction and Associated Vessels implementation measures	Monitor and check	On-going
Complaint Hotline and Email Channel	Construction phase	On-going
Environmental Log Book	Construction phase	On-going

Taking into account the construction works in the reporting period, impact monitoring of air quality, noise, water quality, waste management, landscape & visual, and CWD were carried out in the reporting period.

The EM&A programme also involved weekly site inspections and related auditing conducted by ET for the checking of implementation of required environmental mitigation measures recommended in the approved EIA Report. To promote the environmental awareness and enhance the environmental performance of the contractors, environmental trainings and regular environmental management meetings were conducted during the reporting period which are summarised as below:

- Seven skipper trainings provided by ET;
- Two dolphin observer training sessions provided by ET;
- One environmental briefing on EP and EM&A requirements of the 3RS provided by ET; and
- Forty-five environmental management meetings for EM&A review with works contracts.

The EM&A programme has been following the recommendations presented in the approved EIA Report and the Manual. A summary of implementation status of the environmental mitigation measures for the construction phase of the Project during the reporting period is provided in **Appendix B**.

2 Environmental Monitoring and Auditing

2.1 Air Quality Monitoring

Impact 1-hour Total Suspended Particulates (TSP) monitoring was conducted three times every six days at two representative monitoring stations during the reporting period. The locations of monitoring stations are described in **Table 2.1** and presented in **Figure 2.1**.

2.1.1 Action and Limit Levels

The Action and Limit Levels of the air quality monitoring stipulated in the EM&A programme for triggering the relevant investigation and follow-up procedures under the programme are provided in **Table 2.1** for reference.

Table 2.1: Impact Air Quality Monitoring Stations

Monitoring Station	Location	Action Level ($\mu\text{g}/\text{m}^3$)	Limit Level ($\mu\text{g}/\text{m}^3$)
AR1A	Man Tung Road Park	306	500
AR2	Village House at Tin Sum	298	

2.1.2 Summary of Monitoring Results

The air quality monitoring results in the reporting period are summarised in **Table 2.2** and the graphical plot is presented in **Appendix C**.

Table 2.2: Percentage of Air Quality Monitoring Results within Action and Limit Levels

	AR1A	AR2
Jul 2020	100%	100%
Aug 2020	100%	100%
Sep 2020	100%	100%
Overall	100%	100%

Note: The percentages are calculated by dividing the number of monitoring results within their corresponding Action and Limit Levels by the total number of monitoring results.

All monitoring results were within their corresponding Action and Limit Levels at all monitoring stations in the reporting period.

General meteorological conditions in the last month of the previous quarter and this reporting period were recorded and summarised in **Table 2.3**.

Table 2.3: General Meteorological Condition during Impact Air Quality Monitoring

	Weather	Dominant Wind Direction
Jun 2020	Sunny to Cloudy	Southwest
Jul 2020	Sunny to Cloudy	Southwest
Aug 2020	Sunny to Cloudy	Southeast
Sep 2020	Sunny to Cloudy	Southeast

2.1.3 Conclusion

No dust emission source was observed at the monitoring stations during the monitoring sessions. As the sensitive receivers were far away from the construction activities, with the implementation of dust control measures, there was no adverse impact at the sensitive receivers attributable to the works of the Project.

2.2 Noise Monitoring

Impact noise monitoring was conducted at four representative monitoring stations once per week during 0700 and 1900 in the reporting period. The locations of monitoring stations are described in **Table 2.4** and presented in **Figure 2.1**.

2.2.1 Action and Limit Levels

The Action and Limit Levels of the noise monitoring stipulated in the EM&A programme for triggering the relevant investigation and follow-up procedures under the programme are provided in **Table 2.4** for reference.

Table 2.4: Impact Noise Monitoring Stations

Monitoring Station	Location	Action Level	Limit Level
NM1A	Man Tung Road Park	When one documented complaint is received from any one of the sensitive receivers	75 dB(A)
NM4	Ching Chung Hau Po Woon Primary School		65dB(A) / 70 dB(A) ⁽ⁱ⁾
NM5	Village House in Tin Sum		75 dB(A)
NM6	House No. 1, Sha Lo Wan		75 dB(A)

Note:

⁽ⁱ⁾ The Limit Level for NM4 is reduced to 70dB(A) for being an educational institution. During school examination period, the Limit Level is further reduced to 65dB(A).

2.2.2 Summary of Monitoring Results

The noise monitoring results in the reporting period are summarised in **Table 2.5** and the graphical plot is presented in **Appendix C**.

Table 2.5: Percentage of Noise Monitoring Results within Action and Limit Levels

	NM1A	NM4	NM5	NM6
Jul 2020	100%	100%	100%	100%
Aug 2020	100%	100%	100%	100%
Sep 2020	100%	100%	100%	100%
Overall	100%	100%	100%	100%

Note: The percentages are calculated by dividing the number of monitoring results within their corresponding Action and Limit Levels by the total number of monitoring results.

No complaints were received from any sensitive receiver that triggered the Action Level. All monitoring results were also within the corresponding Limit Levels at all monitoring stations in the reporting period.

General meteorological conditions in the last month of the previous quarter and this reporting period were recorded and summarised in **Table 2.6**.

Table 2.6: General Meteorological Condition during Impact Noise Monitoring

	Weather
Jun 2020	Sunny to Cloudy
Jul 2020	Sunny to Cloudy
Aug 2020	Sunny to Cloudy
Sep 2020	Sunny to Cloudy

2.2.3 Conclusion

Major sources of noise dominating the monitoring stations observed during the construction noise impact monitoring were traffic noise near NM1A and aircraft noise near NM6. As the sensitive receivers were far away from the construction activities, with the implementation of noise control measures, there was no adverse impact at the sensitive receivers attributable to the works of the Project.

2.3 Water Quality Monitoring

During the reporting period, water quality monitoring was conducted three days per week, at mid-flood and mid-ebb tides, at a total of 23 water quality monitoring stations, comprising 12 impact (IM) stations, 8 sensitive receiver (SR) stations, and 3 control (C) stations in the vicinity of the water quality sensitive receivers around the existing airport island in accordance with the Manual. The purpose of water quality monitoring at the IM stations is to promptly capture any potential water quality impacts from the Project before the impacts could become apparent at sensitive receivers (represented by the SR stations). **Table 2.7** describes the details of the monitoring stations. **Figure 2.2** shows the locations of the monitoring stations.

Table 2.7: Monitoring Locations and Parameters for Impact Water Quality Monitoring

Monitoring Station	Description	Coordinates		Parameters
		Easting	Northing	
C1	Control Station	804247	815620	<u>General Parameters</u>
C2	Control Station	806945	825682	DO, pH,
C3 ⁽³⁾	Control Station	817803	822109	Temperature,
IM1	Impact Station	807132	817949	Salinity, Turbidity,
IM2	Impact Station	806166	818163	SS
IM3	Impact Station	805594	818784	<u>DCM Parameters</u>
IM4	Impact Station	804607	819725	Total Alkalinity,
IM5	Impact Station	804867	820735	Heavy Metals ⁽²⁾
IM6	Impact Station	805828	821060	
IM7	Impact Station	806835	821349	
IM8	Impact Station	808140	821830	
IM9	Impact Station	808811	822094	
IM10	Impact Station	809794	822385	
IM11	Impact Station	811460	822057	
IM12	Impact Station	812046	821459	
SR1A ⁽¹⁾	Hong Kong-Zhuhai-Macao Bridge Hong Kong Boundary Crossing Facilities (HKBCF) Seawater Intake for cooling	812660	819977	<u>General Parameters</u>
				DO, pH,
				Temperature,
				Salinity, Turbidity,
				SS

Monitoring Station	Description	Coordinates		Parameters
		Easting	Northing	
SR2 ⁽³⁾	Planned marine park / hard corals at The Brothers / Tai Mo To	814166	821463	<u>General Parameters</u> DO, pH, Temperature, Salinity, Turbidity, SS <u>DCM Parameters</u> Total Alkalinity, Heavy Metals ⁽²⁾⁽⁴⁾
SR3	Sha Chau and Lung Kwu Chau Marine Park / fishing and spawning grounds in North Lantau	807571	822147	<u>General Parameters</u> DO, pH, Temperature, Salinity, Turbidity, SS
SR4A	Sha Lo Wan	807810	817189	
SR5A	San Tau Beach SSSI	810696	816593	
SR6A ⁽⁵⁾	Tai Ho Bay, Near Tai Ho Stream SSSI	814739	817963	
SR7	Ma Wan Fish Culture Zone (FCZ)	823742	823636	
SR8 ⁽⁶⁾	Seawater Intake for cooling at Hong Kong International Airport (East)	811623	820390	

Notes:

- (1) With the operation of HKBCF, water quality monitoring at SR1A station was commenced on 25 October 2018.
- (2) Details of selection criteria for the two heavy metals for regular DCM monitoring refer to the Detailed Plan on Deep Cement Mixing available on the dedicated 3RS website (<http://env.threerunwaysystem.com/en/ep-submissions.html>). DCM specific water quality monitoring parameters (total alkalinity and heavy metals) were only conducted at C1 to C3, SR2, and IM1 to IM12.
- (3) According to the Baseline Water Quality Monitoring Report, C3 station is not adequately representative as a control station of impact/ SR stations during the flood tide. The control reference has been changed from C3 to SR2 from 1 September 2016 onwards.
- (4) Total alkalinity and heavy metals results are collected at SR2 as a control station for regular DCM monitoring.
- (5) As the access to SR6 was obstructed by the construction activities and temporary structures for Tung Chung New Town Extension, the monitoring location has been relocated to SR6A starting from 8 August 2019.
- (6) The monitoring location for SR8 is subject to further changes due to silt curtain arrangements and the progressive relocation of this seawater intake.

2.3.1 Action and Limit Levels

The Action and Limit Levels for general water quality monitoring and regular DCM monitoring stipulated in the EM&A programme for triggering the relevant investigation and follow-up procedures under the programme are presented in **Table 2.8**. The control and IM stations during flood tide and ebb tide for general water quality monitoring and regular DCM monitoring are presented in **Table 2.9**.

Table 2.8: Action and Limit Levels for General Water Quality Monitoring and Regular DCM Monitoring

Parameters	Action Level	Limit Level
Action and Limit Levels for general water quality monitoring and regular DCM monitoring (excluding SR1A & SR8)		
DO in mg/l (Surface, Middle & Bottom)	Surface and Middle 4.5 mg/l	Surface and Middle 4.1 mg/l 5 mg/l for Fish Culture Zone (SR7) only

Parameters	Action Level		Limit Level	
	Bottom 3.4 mg/l		Bottom 2.7 mg/l	
SS in mg/l	23	or 120% of upstream control station at the same tide of the same day, whichever is higher	37	or 130% of upstream control station at the same tide of the same day, whichever is higher
Turbidity in NTU	22.6		36.1	
Total Alkalinity in ppm	95		99	
Representative Heavy Metals for regular DCM monitoring (Chromium)	0.2		0.2	
Representative Heavy Metals for regular DCM monitoring (Nickel)	3.2		3.6	
Action and Limit Levels SR1A				
SS (mg/l)	33		42	
Action and Limit Levels SR8				
SS (mg/l)	52		60	

Notes:

1. For DO measurement, Action or Limit Level is triggered when monitoring result is lower than the limits.
2. For parameters other than DO, Action or Limit Level of water quality results is triggered when monitoring results is higher than the limits.
3. Depth-averaged results are used unless specified otherwise.
4. Details of selection criteria for the two heavy metals for regular DCM monitoring refer to the Detailed Plan on Deep Cement Mixing available on the dedicated 3RS website <http://env.threerunwaysystem.com/en/ep-submissions.html>
5. The Action and Limit Levels for the two representative heavy metals chosen will be the same as that for the intensive DCM monitoring.

Table 2.9: The Control and Impact Stations during Flood Tide and Ebb Tide for General Water Quality Monitoring and Regular DCM Monitoring

Control Station	Impact Stations
Flood Tide	
C1	IM1, IM2, IM3, IM4, IM5, IM6, IM7, IM8, SR3
SR2 ¹	IM7, IM8, IM9, IM10, IM11, IM12, SR1A, SR3, SR4A, SR5A, SR6A, SR8
Ebb Tide	
C1	SR4A, SR5A, SR6A
C2	IM1, IM2, IM3, IM4, IM5, IM6, IM7, IM8, IM9, IM10, IM11, IM12, SR1A, SR2, SR3, SR7, SR8

Note:

1. As per findings of Baseline Water Quality Monitoring Report, the control reference has been changed from C3 to SR2 from 1 Sep 2016 onwards.

2.3.2 Summary of Monitoring Results

The summary or results within their corresponding Action and Limit Levels in the reporting period are presented in **Table 2.10**. The weather and sea conditions in the last month of the previous quarter and this reporting period were recorded and summarised in **Table 2.11**.

Table 2.10 : Percentage of Water Quality Monitoring Results within Action and Limit Levels

	General Water Quality Monitoring				Regular DCM Monitoring		
	DO (Surface and Middle)	DO (Bottom)	SS	Turbidity	Alkalinity	Chromium	Nickel
Jul 2020	97.8% (445/455)	97.1% (442/455)	100% (507/507)	100% (455/455)	100% (312/312)	100% (312/312)	100% (312/312)
Aug 2020	99.5% (418/420)	98.8% (415/420)	100% (468/468)	100% (420/420)	100% (288/288)	100% (288/288)	100% (288/288)
Sep 2020	100% (438/438)	100% (438/438)	100% (488/488)	100% (438/438)	100% (300/300)	100% (300/300)	100% (300/300)
Overall	99.1%	98.6%	100%	100%	100%	100%	100%

Note: The percentages are calculated by dividing the number of depth-averaged results complying with their corresponding Action and Limit Levels by the total number of depth-averaged results.

Table 2.11: General Weather Condition and Sea Condition during Impact Water Quality Monitoring

	Weather	Sea Condition
Jun 2020	Sunny to Rainy	Calm to Rough
Jul 2020	Sunny to Rainy	Calm to Rough
Aug 2020	Sunny to Rainy	Calm to Rough
Sep 2020	Sunny to Rainy	Calm to Rough

The monitoring results for all parameters, except DO, obtained during the reporting period were within their corresponding Action and Limit Levels stipulated in the EM&A programme. Relevant investigation and follow-up actions will be conducted according to the EM&A programme if the corresponding Action and Limit Levels are triggered.

For DO, some of the testing results triggered the relevant Action or Limit Level in the reporting period, and investigations were conducted accordingly. Summaries of results triggering Action or Limit Level for DO are presented **Table 2.12** to **Table 2.15**.

Details of the investigation findings were presented in Construction Phase Monthly EM&A Report Nos. 55 and 56, which concluded that all results triggering the Action or Limit Level were not related to the Project.

Table 2.12: Summary of DO (Surface and Middle) Results triggering Action or Limit Level (Mid-Ebb Tide)

	IM1	IM2	IM3	IM4	IM5	IM6	IM7	IM8	IM9	IM10	IM11	IM12	SR2	SR3	SR4A	SR5A	SR6A	SR7
21/07/2020		D	D	D											D			
15/08/2020			D															
No. of result triggering Action or Limit Level	0	1	2	1	1	1	0	0	0	0	0	0	0	0	1	0	0	0

Table 2.13: Summary of DO (Bottom) Results triggering Action or Limit Level (Mid-Ebb Tide)

	IM1	IM2	IM3	IM4	IM5	IM6	IM7	IM8	IM9	IM10	IM11	IM12	SR2	SR3	SR4A	SR5A	SR6A	SR7
16/07/2020		D	D	D														
18/07/2020	D	D	D	D											D			
15/08/2020	D	D	D	D											D			
No. of result triggering Action or Limit Level	2	3	3	3	0	0	0	0	0	0	0	0	0	1	2	0	0	0

Table 2.14: Summary of DO (Surface and Middle) Results triggering Action or Limit Level (Mid-Flood Tide)

	IM1	IM2	IM3	IM4	IM5	IM6	IM7	IM8	IM9	IM10	IM11	IM12	SR3	SR4A	SR5A	SR6A	SR7
21/07/2020	D		D	D	D												
22/08/2020																	
No. of result triggering Action or Limit Level	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	1

Table 2.15: Summary of DO (Bottom) Results triggering Action or Limit Level (Mid-Flood Tide)

	IM1	IM2	IM3	IM4	IM5	IM6	IM7	IM8	IM9	IM10	IM11	IM12	SR3	SR4A	SR5A	SR6A	SR7
16/07/2020			D	D													
18/07/2020			D														
21/07/2020				D													
No. of result triggering Action or Limit Level	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0

Note: The monitoring results compiled with their corresponding Action or Limit Levels are presented in **Appendix C**.

Legend:	
	Result within corresponding Action and Limit Levels
	Result triggered the Action Level at monitoring station located upstream of the Project based on dominant tidal flow
D	Result triggered the Action Level at monitoring station located downstream of the Project based on dominant tidal flow
	Result triggered the Limit Level at monitoring station located upstream of the Project based on dominant tidal flow
D	Result triggered the Limit Level at monitoring station located downstream of the Project based on dominant tidal flow
	Upstream station with respect to the Project during the respective tide based on dominant tidal flow
	Downstream station with respect to the Project during the respective tide based on dominant tidal flow

2.3.3 Conclusion

In the reporting period, it is noted that most monitoring results were within their corresponding Action and Limit Levels, while only a minor number of results triggered their corresponding Action or Limit Level, and investigations were conducted accordingly. Based on the findings presented in Construction Phase Monthly EM&A Report Nos. 55 and 56, all cases that triggered the corresponding Action or Limit Level were not related to the Project. Hence, the Project did not

introduce adverse impact to all water quality sensitive receivers. All required actions under the Event and Action Plan were followed.

Nevertheless, the non-project related triggers were attended to and initiated corresponding action and measures. As part of the EM&A programme, the construction methods and mitigation measures for water quality will continue to be monitored and opportunities for further enhancement will continue to be explored and implemented where possible, to strive for better protection of water quality and the marine environment.

In the meantime, the contractors were reminded to implement and maintain all mitigation measures during weekly site inspections and regular environmental management meetings. These include maintaining mitigation measures properly for reclamation works including DCM works, marine filling, and seawall construction as recommended in the Manual.

2.4 Waste Monitoring

In accordance with the Manual, waste generated from construction activities was audited once per week to determine if wastes were being managed in accordance with the Waste Management Plan (WMP) prepared for the Project, contract-specific WMP, and any statutory and contractual requirements. All aspects of waste management including waste generation, storage, transportation, and disposal were assessed during the audits.

2.4.1 Action and Limit Levels

The Action and Limit Levels of the construction waste are provided in **Table 2.16**.

Table 2.16: Action and Limit Levels for Construction Waste

Monitoring Stations	Action Level	Limit Level
Construction Area	When one valid documented complaint is received	Non-compliance of the WMP, contract-specific WMPs, any statutory and contractual requirements

2.4.2 Summary of Monitoring Results

Weekly monitoring of the Project construction works was carried out by the ET in the reporting period to check and monitor the implementation of proper waste management practices.

Recommendations made by the ET included provision and maintenance of proper chemical waste storage area, as well as handling, segregation, and regular disposal of general refuse. The contractors had taken actions to implement the recommended measures. Waste management audits were carried out by ET according to the requirement of the Waste Management Plan, Updated EM&A Manual and the implementation schedule of the waste management mitigation measures in **Appendix B**.

Based on updated contractors' information, summary of construction waste generated in the reporting period is presented in **Table 2.17**. Proactive measures have been undertaken during the re-configuration of T2 building. The contractor has established the recycling strategy for C&D materials with proper planning and design to maximize recycling and reuse. Dedicated recyclers were employed for different kinds of recyclable materials by the contractor, and ET and IEC have carried out site visit to recyclers' facilities to review recycling process. Recycling materials before leaving the site are weighted by a weight bridge and monitored by CCTV system. Dedicated areas for sorting of materials are established on site. Recyclable materials such as steel, reinforcement bar, structural steel, aluminium, copper, other metals and glass are sorted on-site and transported

off-site for recycling. ET and IEC have carried out site audits regularly and reviewed the trip ticket system.

Table 2.17: Construction Waste Statistics

	C&D ⁽¹⁾ Material Stockpiled for Reuse or Recycle (m ³)	C&D Material Reused in the Project (m ³)	C&D Material Reused in other Projects (m ³)	C&D Material Transferred to Public Fill ⁽³⁾ (m ³)	Chemical Waste (kg)	Chemical Waste (l)	General Refuse (tonne)
Jul 2020 ⁽²⁾	2,895	43,002	16	3,785	60	8,200	1,035
Aug 2020 ⁽²⁾	6,005	70,022	0	2,735	0	1,400	1,224
Sep 2020 ⁽²⁾	5,822	101,504	1,952	1,842	50	9,000	1,534
Total	14,722	214,528	1,968	8,362	110	18,600	3,792

Notes:

1. C&D refers to Construction and Demolition.
2. Paper, metals and/or plastics were recycled in the reporting period.
3. C&D materials not suitable for reuse on-site, including asphalt waste and sediment slurry, were transferred to public fill during the reporting period.

There were no complaints, non-compliance of the WMP, contract-specific WMPs, statutory and contractual requirements that triggered Action and Limit Levels in the reporting period.

2.4.3 Marine Sediment Management

Marine sediment is managed according to the EIA Report, Updated EM&A Manual and Waste Management Plan of the Project. The sampling process, storage conditions of the excavated marine sediment, treatment process, final backfilling location as well as associated records were inspected and checked by ET and verified by IEC to ensure they were in compliance with the requirements as stipulated in the Waste Management Plan.

Sampling works for marine sediment generated from the reclaimed land area was on-going during the reporting period.

2.5 Chinese White Dolphin Monitoring

CWD monitoring was conducted by vessel line transect survey at a frequency of two full surveys per month, supplemented by land-based theodolite tracking survey and PAM. The frequency of the land-based theodolite tracking survey during the construction phase was one day per month at both Sha Chau (SC) and Lung Kwu Chau (LKC) stations, as stipulated in the Manual. The vessel survey transects followed the transect lines proposed in the Manual and are consistent with those used in the Agriculture, Fisheries and Conservation Department (AFCD) long-term CWD monitoring programme. The transect locations of CWD monitoring by vessel line transect survey conducted from July to September 2020 are shown in **Figure 2.3**, whilst the land-based theodolite tracking survey stations are described in **Table 2.18** and depicted in **Figure 2.4**. The location of the PAM device is shown in **Figure 2.10**.

Table 2.18: Land-based Theodolite Tracking Survey Station Details

Stations	Location	Geographical Coordinates	Station Height (m)	Approximate Tracking Distance (km)
D	Sha Chau (SC)	22° 20' 43.5" N 113° 53' 24.66" E	45.66	2
E	Lung Kwu Chau (LKC)	22° 22' 44.83" N 113° 53' 0.2" E	70.40	3

2.5.1 Action and Limit Levels

The Action Level and Limit Level for CWD monitoring were formulated by an action response approach using the running quarterly dolphin encounter rates (STG and ANI) derived from baseline monitoring data, as presented in the CWD Baseline Monitoring Report. The derived values of Action and Limit Levels for CWD monitoring are shown in **Table 2.19**.

Table 2.19: Derived Values of Action Level and Limit Level for Chinese White Dolphin Monitoring

NEL, NWL, AW, WL and SWL as a Whole	
Action Level	Running quarterly STG < 1.86 & ANI < 9.35
Limit Level	Two consecutive running quarterly (3-month) STG < 1.86 & ANI < 9.35

2.5.2 Summary of Monitoring Results

2.5.2.1 Vessel Line Transect Survey

Survey Effort

During the July to September 2020 reporting period, a total of six sets of vessel line transect survey covering all transects in Northeast Lantau (NEL), Northwest Lantau (NWL), Airport West (AW), West Lantau (WL) and Southwest Lantau (SWL) survey areas were conducted at a frequency of twice per month, in each survey area.

A total of around 1,336 km of survey effort was collected from these surveys, with around 92.8% of the total survey effort being conducted under favourable weather condition (i.e. Beaufort Sea State 3 or below with favourable visibility). Details of the survey effort data are presented in **Appendix C**.

CWD Sighting

From July to September 2020, there were a total of 42 sightings of CWDs, with 176 dolphins sighted (**Table 2.20**). Amongst these sightings, 40 sightings with 164 dolphins were recorded during on-effort searches under favourable weather condition.

When breaking down the sightings by survey areas, one sighting with 2 dolphins was recorded in NWL, 24 sightings with 124 dolphins and 17 sightings with 50 dolphins were recorded in WL and SWL survey areas respectively during the current reporting period. No CWD was sighted in NEL survey area or on AW transects.

Compared with the last quarter (i.e. April to June 2020), the total number of CWD sightings and the total number of dolphins, as well as the breakdowns of these numbers by survey areas remain similar in this quarter.

Compared with the same quarter of last year in 2019 (i.e. July to September 2019), there is an obvious decline in terms of both the total number of CWD sightings and the total number of dolphins, by 39% and 38% respectively.

Table 2.20 below shows the comparison of the numbers of sightings and dolphins amongst the current reporting period, last quarter, and the same quarter of last year.

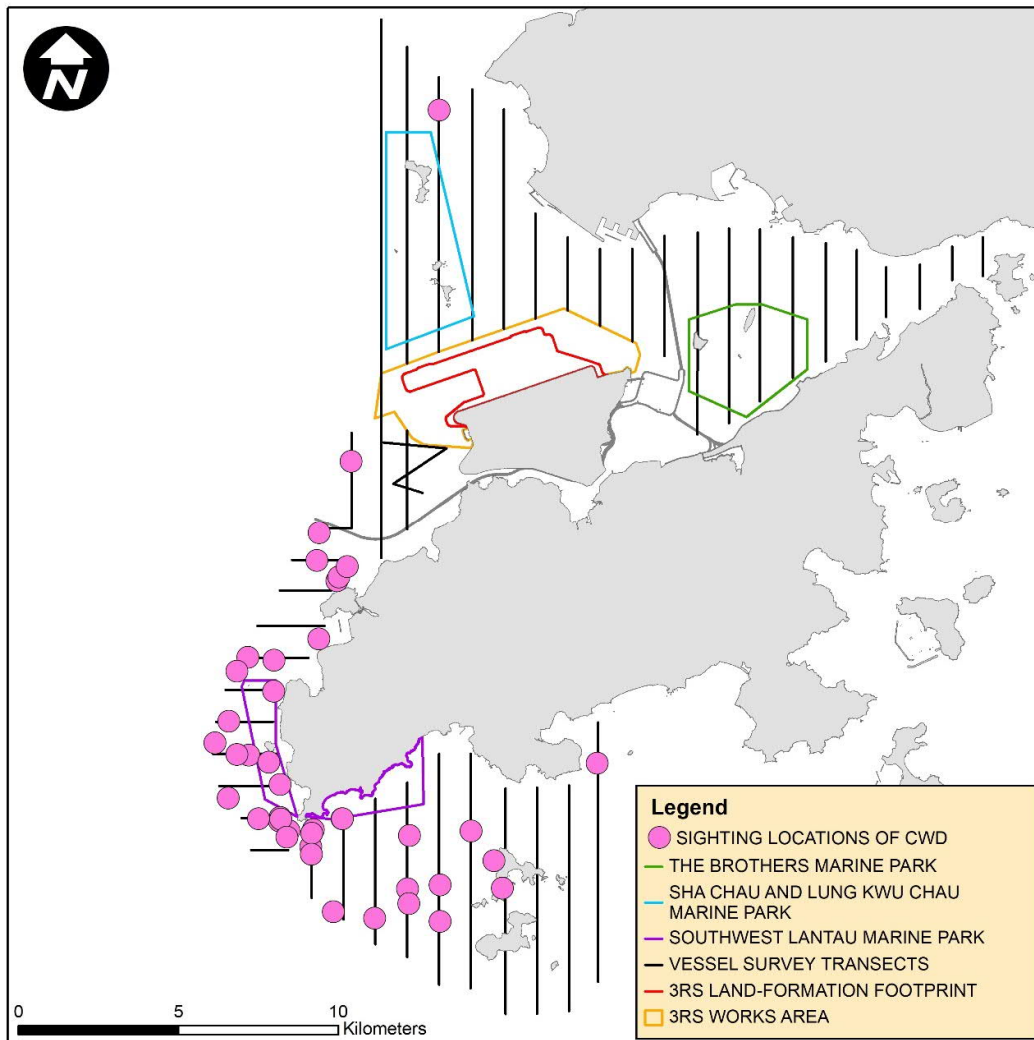
Table 2.20: Summary of Number of CWD Sightings and Number of Dolphins for the Same Quarter Last Year, Previous Quarter, and Current Reporting Period

	Same Quarter of Last Year	Previous Reporting Period	Current Reporting Period
	July to September 2019	April to June 2020	July to September 2020
NEL	0 (0)	0 (0)	0 (0)
NWL	3 (4)	0 (0)	1 (2)
AW	0 (0)	0 (0)	0 (0)
WL	41 (182)	26 (121)	24 (124)
SWL	25 (97)	15 (57)	17 (50)
Total	69 (283)	41 (178)	42 (176)

Note: Values in () represent number of dolphins

The distribution of CWD sightings recorded from July to September 2020 is illustrated in **Figure 2.5**. In NWL, the CWD sighting was recorded north off the Sha Chau and Lung Kwu Chau Marine Park. In WL, most of the CWD sightings clustered at several locations including Tai O, Yi O, Peaked Hill and Fan Lau. In SWL, some dolphin sightings were scattered at waters west of the Soko Islands while others were clustered at Fan Lau. No CWD sightings were recorded in NEL or AW survey areas. Details of the sighting data are presented in **Appendix C**.

Figure 2.5: Sightings Distribution of Chinese White Dolphins from July to September 2020



Remarks: (1) Please note that there are 42 pink circles on the map indicating the sighting locations of CWD. Some of them were very close to each other and therefore appear overlapped on this sighting distribution map. (2) Marine park excludes land area and the landward boundary generally follows the high water mark along the coastline.

Encounter Rate

The dolphin encounter rates for the number of on-effort dolphin sightings per 100 km survey effort (STG) and for the total on-effort number of dolphins per 100 km survey effort (ANI) in the whole survey area (i.e. NEL, NWL, AW, WL and SWL) for July, August and September 2020 are summarised in **Table 2.21**.

In this reporting period, both the monthly and running quarterly STGs and ANIs declined drastically from July to September. No Action Level for CWD monitoring was triggered during the reporting period.

Compared with the previous reporting period (i.e. April to June 2020), both the running quarterly STG and ANI remain relatively steady. While comparing with the same quarter of last year (i.e. July to September 2019), both the running quarterly STG and ANI decrease significantly, from

5.36 to 3.23 and from 21.98 to 13.23 respectively. Encounter rates for these periods are summarised in **Table 2.21** and graphical presentation is provided in **Appendix C**.

Table 2.21: Summary of Monthly and Running Quarterly STG and ANI of Chinese White Dolphin for the Same Quarter Last Year, Previous Quarter, and Current Reporting Period

	Same Quarter of Last Year			Previous Reporting Period			Current Reporting Period		
	Jul 19	Aug 19	Sep 19	Apr 20	May 20	Jun 20	Jul 20	Aug 20	Sep 20
Monthly STG	7.64	3.95	4.47	1.58	2.12	5.86	6.84	1.90	1.18
Monthly ANI	31.72	16.50	17.63	6.77	13.91	21.47	28.62	7.59	4.48
Running Quarterly STG	4.37	4.92	5.36	2.36	2.03	3.13	4.88	4.81	3.23
Running Quarterly ANI	17.72	19.69	21.98	7.63	9.45	13.86	21.16	19.01	13.23

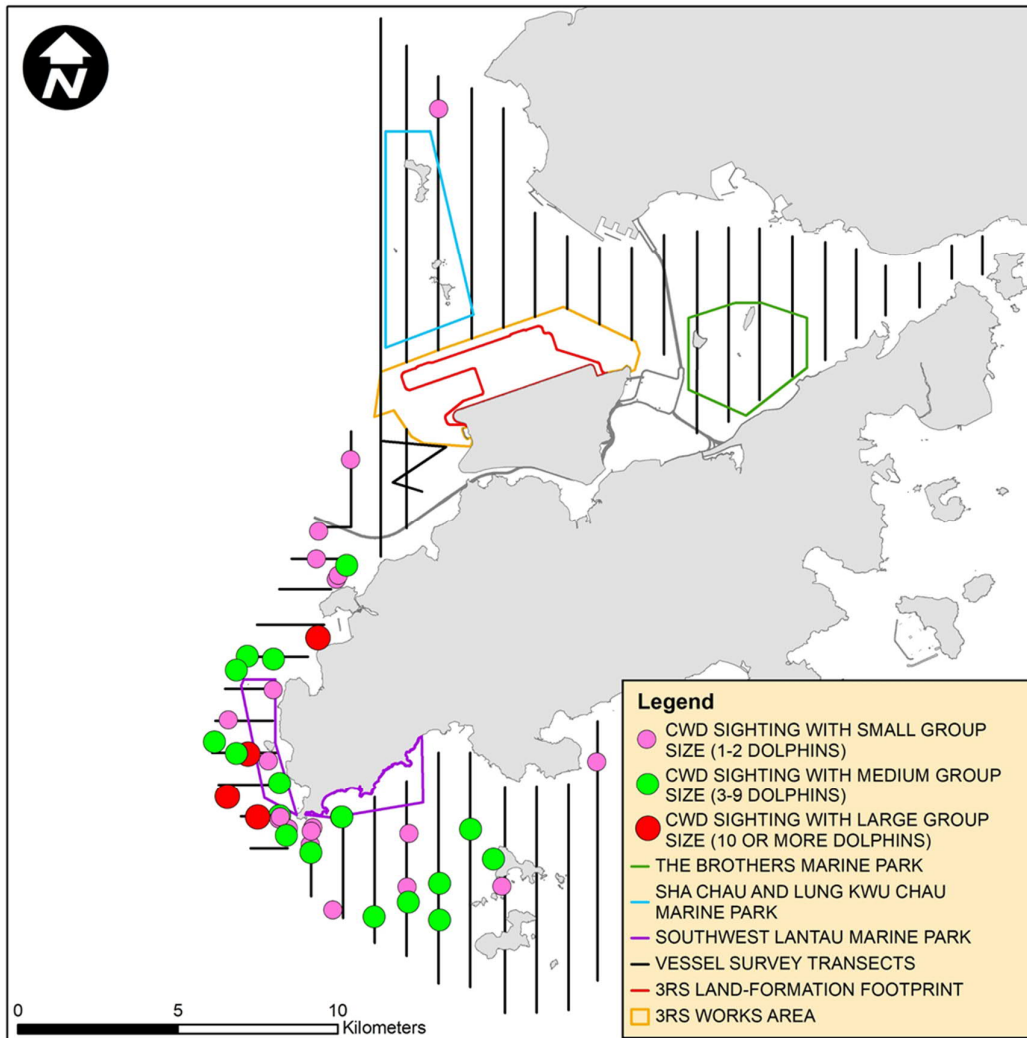
Note: For detailed calculations of encounter rates STG and ANI for the current reporting period, please refer to the Construction Phase Monthly EM&A Report Nos. 55, 56 and 57.

Group Size

Between July and September 2020, the group size of CWD sightings ranged from 1 to 19 dolphins. The average group size of CWDs was 4.2 dolphins per group, which is slightly smaller than that of the last quarter (4.3 dolphins per group). The average group size of CWDs in this reporting quarter is slightly larger than that of the same quarter of last year (4.1 dolphins per group).

In this reporting quarter, the number of CWD sightings with medium group size (i.e. 3-9 dolphins) was slightly lower than that with small group size (i.e. 1-2 dolphins). There were four CWD sightings with large group size (i.e. 10 or more dolphins) recorded in this reporting period.

There were no distinct distribution patterns of small-sized and medium-sized CWD groups observed in the current reporting period. However, all large-sized CWD groups were recorded in WL. Sighting locations of CWD groups with different group sizes are depicted in **Figure 2.6**.

Figure 2.6: Sighting Locations of Chinese White Dolphins with Different Group Sizes

Remarks: (1) Please note that there are 42 circles on the map indicating the sighting locations of CWD. Some of them were very close to each other and therefore appear overlapped on this sighting distribution map. (2) Marine park excludes land area and the landward boundary generally follows the high water mark along the coastline.

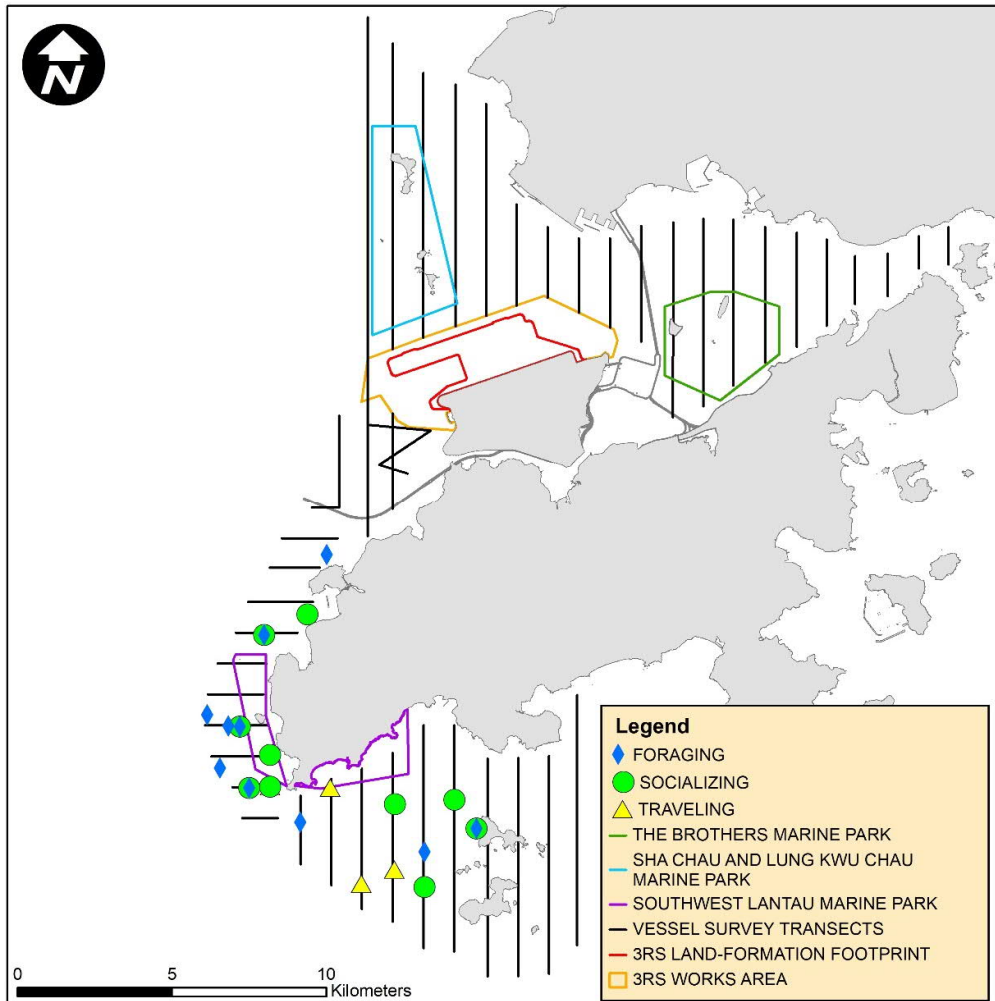
Activities and Association with Fishing Boats

From July to September 2020, 10 sightings of CWDs were recorded with feeding activities. Amongst these sightings, one was observed in association with operating purse seiner.

The number of sightings with feeding recorded in the current reporting period is slightly lower than that in the previous reporting period (i.e. 11 sightings involved feeding activities in which four sightings associated with fishing boat between April and June 2020). The number of CWD sightings with feeding activities is lower than that in the same quarter of last year (i.e. 18 sightings between July and September 2019).

The sighting locations of CWDs engaged in different behaviours during the current reporting period are illustrated in **Figure 2.7**.

Figure 2.7: Sighting Locations of Chinese White Dolphins Engaged in Different Behaviours

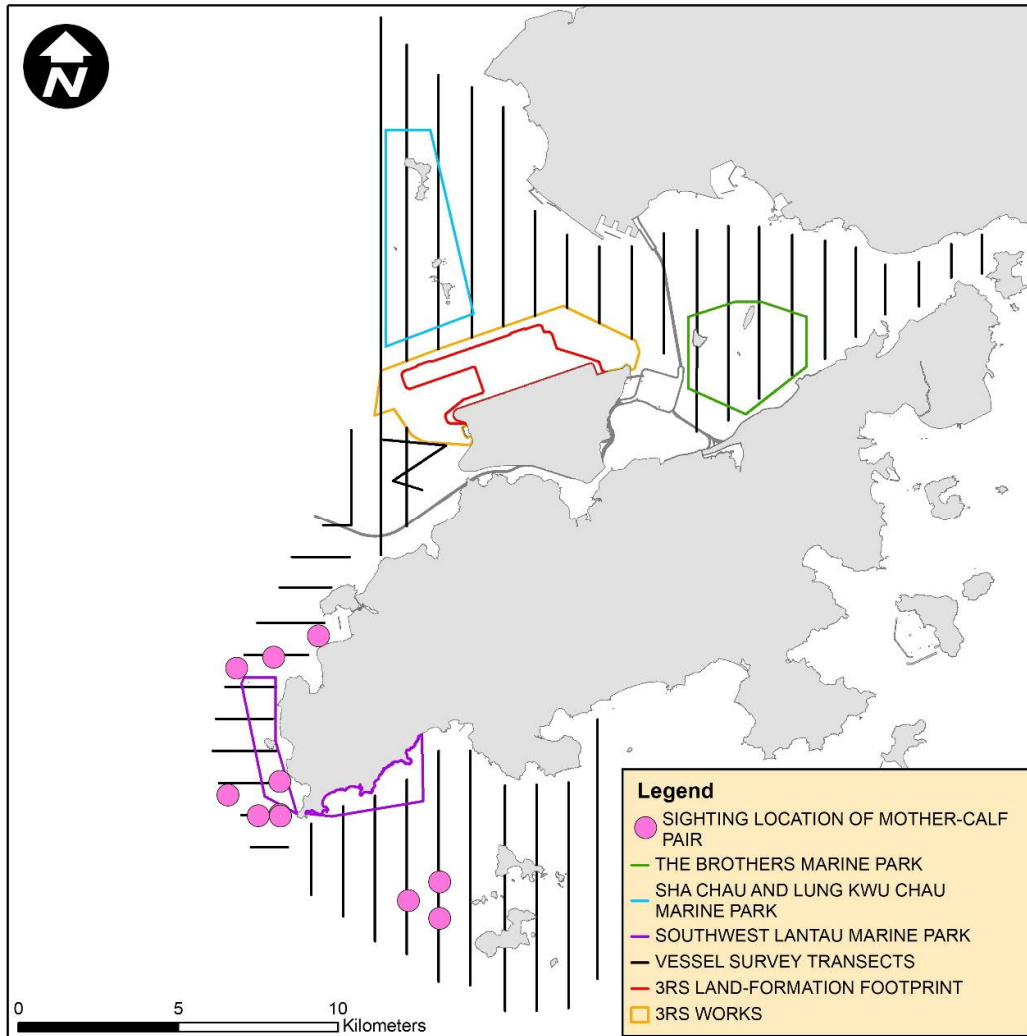


Remarks: Marine park excludes land area and the landward boundary generally follows the high water mark along the coastline.

Mother-calf Pairs

From July to September 2020, 11 sightings of CWDs were recorded with the presence of mother-and-unsponsored calf and/or mother-and-unsponsored juvenile pair, which is more than that recorded in the previous reporting quarter (i.e. seven sightings in April to June 2020). The number is the same as that recorded in the same quarter of last year (i.e. 11 sightings in July to September 2019). All these sightings were recorded in WL except one recorded in SWL.

The locations of CWD sightings with the presence of mother-and-calf and/or mother-and-unsponsored juvenile are shown in **Figure 2.8**.

Figure 2.8: Sighting Locations of Mother-calf Pairs

Remarks: (1) Please note that there are 11 circles on the map indicating the sighting locations of Mother-Calf pair. (2) Marine park excludes land area and the landward boundary generally follows the high water mark along the coastline.

Photo Identification

During July to September 2020, a total number of 77 different CWD individuals were identified altogether for 110 times. Re-sighting information of CWD individuals provides an initial idea of their range use and apparent connection between different areas of Lantau waters. Amongst these 77 different CWD individuals, 25 animals (i.e. NLMM023, NLMM037, NLMM052, NLMM060, SLMM003, SLMM007, SLMM010, SLMM012, SLMM014, SLMM025, SLMM037, SLMM049, SLMM052, SLMM060, SLMM062, WLMM001, WLMM008, WLMM009, WLMM030, WLMM079, WLMM085, WLMM091, WLMM092, WLMM131 and WLMM147) were sighted for more than once.

Eleven individuals including NLMM052, SLMM003, SLMM012, SLMM025, SLMM037, SLMM049, WLMM008, WLMM009, WLMM079, WLMM131 and WLMM147 were re-sighted in different survey areas during this reporting period. All cross-area movements were between WL and SWL

survey areas. The most frequently re-sighted individual in this reporting quarter was WLMM079 which has been encountered altogether for five times. The number of CWD individuals re-sighted more than once and the number of CWD individuals showing cross-area movement in the current reporting period are both lower than those of the previous reporting quarter from April to June 2020 (34 and 14 individuals respectively).

A summary of photo identification works is presented in **Table 2.22**. Representative photos of the 77 identified individuals and figures depicting the sighting locations of the aforementioned 25 re-sighted individuals recorded in this reporting period are presented **Appendix C**.

Table 2.22: Summary of Photo Identification

Individual ID	Date of sighting	Sighting Group No.	Area	Individual ID	Date of sighting	Sighting Group No.	Area
NLMM012	13-Jul-20	4	WL	WLMM019	22-Jul-20	1	NWL
NLMM013	13-Jul-20	5	WL	WLMM028	20-Jul-20	4	SWL
NLMM015	15-Sep-20	2	WL	WLMM029	20-Jul-20	4	SWL
NLMM019	20-Jul-20	4	SWL	WLMM030	06-Jul-20	2	WL
NLMM020	06-Jul-20	4	WL		13-Jul-20	2	WL
NLMM023	09-Sep-20	1	WL	WLMM038	13-Jul-20	1	WL
	15-Sep-20	1	WL	WLMM043	13-Jul-20	5	WL
NLMM027	20-Jul-20	3	SWL	WLMM047	13-Jul-20	7	WL
NLMM034	13-Jul-20	1	WL	WLMM056	09-Sep-20	2	WL
NLMM037	20-Jul-20	8	SWL	WLMM060	13-Jul-20	6	WL
		9	SWL	WLMM062	17-Aug-20	1	WL
NLMM039	07-Aug-20	2	WL	WLMM063	21-Jul-20	3	SWL
NLMM043	09-Sep-20	1	WL	WLMM068	17-Aug-20	1	WL
NLMM052	13-Jul-20	5	WL	WLMM071	06-Jul-20	2	WL
	21-Jul-20	5	SWL	WLMM073	07-Aug-20	4	WL
NLMM060	06-Jul-20	2	WL	WLMM079	13-Jul-20	7	WL
	17-Aug-20	1	WL		20-Jul-20	5	SWL
NLMM063	22-Jul-20	1	NWL		21-Jul-20	4	SWL
SLMM003	13-Jul-20	7	WL		09-Sep-20	2	WL
	21-Jul-20	4	SWL		15-Sep-20	3	WL
	09-Sep-20	2	WL	WLMM080	13-Jul-20	6	WL
	15-Sep-20	3	WL	WLMM085	13-Jul-20	6	WL
SLMM007	13-Jul-20	6	WL			7	WL
		7	WL	WLMM086	13-Jul-20	2	WL
SLMM010	13-Jul-20	6	WL	WLMM089	13-Jul-20	5	WL
	07-Aug-20	4	WL	WLMM091	13-Jul-20	2	WL
SLMM012	21-Jul-20	2	SWL			3	WL
	09-Sep-20	2	WL	WLMM092	13-Jul-20	2	WL
SLMM014	06-Jul-20	3	WL			3	WL
	07-Aug-20	4	WL	WLMM095	17-Aug-20	1	WL
SLMM025	13-Jul-20	7	WL	WLMM098	13-Jul-20	1	WL
	20-Jul-20	3	SWL	WLMM102	06-Jul-20	1	WL
SLMM027	21-Jul-20	5	SWL	WLMM103	17-Aug-20	1	WL
SLMM037	20-Jul-20	5	SWL	WLMM107	06-Jul-20	2	WL
	09-Sep-20	2	WL	WLMM109	07-Aug-20	4	WL
SLMM045	15-Sep-20	2	WL	WLMM112	13-Jul-20	6	WL
SLMM049	13-Jul-20	7	WL	WLMM122	21-Jul-20	7	SWL
	20-Jul-20	5	SWL	WLMM131	07-Aug-20	3	WL
	07-Aug-20	4	WL		10-Aug-20	2	SWL
SLMM052	13-Jul-20	7	WL			3	SWL

Individual ID	Date of sighting	Sighting Group No.	Area	Individual ID	Date of sighting	Sighting Group No.	Area
	07-Aug-20	4	WL	WLMM133	17-Aug-20	1	WL
SLMM060	20-Jul-20	1	SWL	WLMM136	06-Jul-20	2	WL
	21-Jul-20	7	SWL	WLMM147	13-Jul-20	7	WL
SLMM062	20-Jul-20	3	SWL		20-Jul-20	5	SWL
	21-Jul-20	2	SWL		21-Jul-20	4	SWL
SLMM070	21-Jul-20	2	SWL	WLMM149	06-Jul-20	2	WL
SLMM072	09-Sep-20	2	WL	WLMM154	06-Jul-20	2	WL
WLMM001	13-Jul-20	6	WL	WLMM155	13-Jul-20	1	WL
		7	WL	WLMM156	13-Jul-20	1	WL
WLMM007	07-Aug-20	4	WL	WLMM157	13-Jul-20	7	WL
WLMM008	21-Jul-20	5	SWL	WLMM158	13-Jul-20	7	WL
	07-Aug-20	4	WL	WLMM159	13-Jul-20	7	WL
WLMM009	13-Jul-20	6	WL	WLMM160	07-Aug-20	4	WL
	20-Jul-20	4	SWL	WLMM161	17-Aug-20	1	WL
WLMM013	15-Sep-20	1	WL	WLMM162	17-Aug-20	1	WL

2.5.2.2 Land-based Theodolite Tracking Survey

Survey Effort

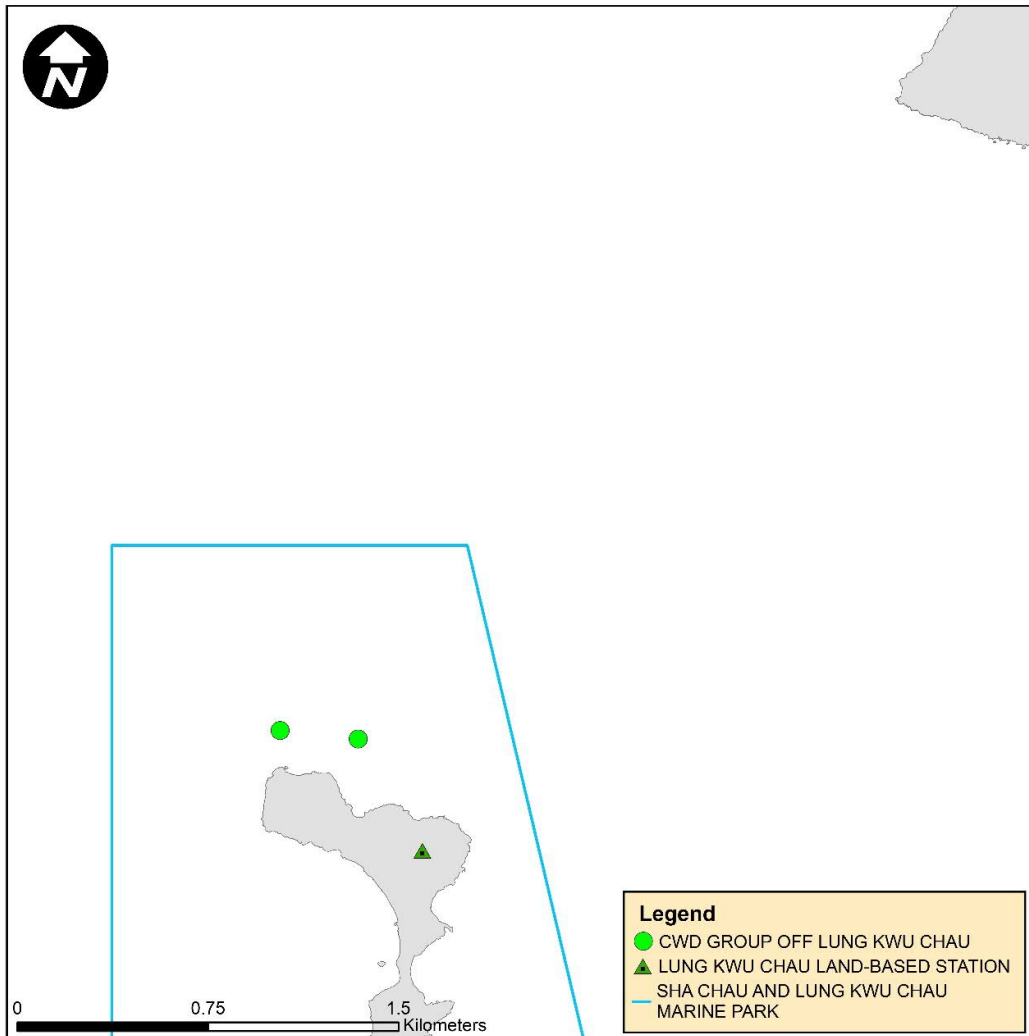
During July to September 2020, a total of six days of land-based theodolite tracking survey effort were completed, including three days on Lung Kwu Chau and three days on Sha Chau. In total, two CWD groups were tracked from the Lung Kwu Chau station while no CWD groups were tracked from the Sha Chau station, with an overall 0.06 CWD groups sighted per survey hour.

Information on survey effort and CWD groups sighted during land-based theodolite tracking surveys are presented in **Table 2.23**. Details on the survey effort and CWD groups tracked are presented in **Appendix C**. The first sighting locations of CWD groups tracked between July and September 2020 are shown in **Figure 2.9**.

Table 2.23: Summary of Survey Effort and CWD Group of Land-based Theodolite Tracking Survey

Land-based Station	# of Survey Sessions	Survey Effort (hh:mm)	# CWD Groups Sighted	CWD Group Sighting per Survey Hour
July 2020				
Lung Kwu Chau	1	06:00	0	0
Sha Chau	1	06:00	0	0
TOTAL	2	12:00	0	0
August 2020				
Lung Kwu Chau	1	06:00	2	0.33
Sha Chau	1	06:00	0	0
TOTAL	2	12:00	2	0.17
September 2020				
Lung Kwu Chau	1	06:00	0	0
Sha Chau	1	06:00	0	0
TOTAL	2	12:00	0	0
OVERALL	6	36:00	2	0.06

Figure 2.9: Plots of First Sightings of All CWD Groups from Land-based Stations



Remark: Marine park excludes land area and the landward boundary generally follows the high water mark along the coastline.

2.5.2.3 Progress Update on PAM

An Ecological Acoustic Recorder (EAR) has been deployed and positioned to the south of Sha Chau Island inside the SCLKCMP (**Figure 2.10**) with 20% duty cycle, while data from the EAR intended primarily to supplement the data collected from the land-based theodolite tracking survey station on Sha Chau. In this reporting period, the EAR has been retrieved on 22 July and 3 September 2020 for data collection and subsequently redeployed. The EAR deployment is generally for 6 weeks prior to data retrieval for analysis. As the period of data collection and analysis takes more than four months, PAM results could not be reported in quarterly intervals but report for supplementing the annual CWD monitoring analysis.

2.5.2.4 Site Audit for CWD-related Mitigation Measures

During the reporting period, silt curtains were in place by the contractors for marine filling works (similar to the previous reporting period), in which dolphin observers were deployed by each contractor in accordance with the Marine Mammal Watching Plan (MMWP). Teams of at least two dolphin observers were deployed at 2 to 6 dolphin observation stations by the contractors for continuous monitoring of the DEZ by all contractors for DCM works and seawall construction that were similar to the previous reporting period in accordance with the DEZ Plan. Trainings for the proposed dolphin observers on the implementation of MMWP and DEZ monitoring were provided by the ET prior to the aforementioned works, with a cumulative total of 698 individuals being trained and the training records were kept by the ET. From the contractors' MMWP observation records and DEZ monitoring records, no dolphin or other marine mammals were observed within or around the silt curtains or the DEZ in this reporting period. The contractors' records were also audited by the ET during site inspection.

Audits of acoustic decoupling for construction vessels were carried out during weekly site inspection and summarised in **Section 2.6**. Summary of audits of SkyPier HSFs route diversion and speed control and construction vessel management are presented in **Section 2.7** and **Section 2.8** respectively.

2.6 Environmental Site Inspection

Site inspections of the construction works were carried out on a weekly basis to monitor the implementation of proper environmental pollution control and mitigation measures for the Project. Bi-weekly site inspections were also conducted by the IEC. Besides, *ad-hoc* site inspections were conducted by ET and IEC if environmental problems were identified, or subsequent to receipt of an environmental complaint, or as part of the investigation work. These site inspections provided a direct means to reinforce the specified environmental protection requirements and pollution control measures in construction sites.

During site inspections, environmental situation, status of implementation of pollution control and mitigation measures were observed. Environmental documents and site records, including waste disposal record, maintenance record of environmental equipment, and relevant environmental permit and licences, were also checked on-site. Observations were recorded in the site inspection checklist and passed to the contractor together with the appropriate recommended mitigation measures where necessary in order to advise contractors on environmental improvement, awareness and on-site enhancement measures. The observations were made with reference to the following information during the site inspections:

- The EIA and EM&A requirements;
- Relevant environmental protection laws, guidelines, and practice notes;
- The EP conditions and other submissions under the EP;
- Monitoring results of EM&A programme;
- Works progress and programme;
- Proposal of individual works;
- Contract specifications on environmental protection; and
- Previous site inspection results.

Good site practices were implemented in the project to enhance environmental performance. Key examples are highlighted as below:

1. Wasted lead acid batteries dismantled from APM trains were collected for recycling by a battery recycler to save the landfill space, eliminate the risk of pollution to the environment, and minimize the wastage.

2. Expired safety helmets were collected and delivered to a local recycler to facilitate the recovery of plastic.
3. Dust suppression control management plan for the new reclaimed land area was devised which consisted of the travelling of water browsers along the designated vehicle travelling routes, and establishment of water mist canon for the dust suppression during the handling of construction materials.

		
<p>Wasted lead acid batteries recycling via local battery recycler</p>	<p>Plastic waste recovery from the expired safety helmets</p>	<p>Dust suppression control management plan</p>

Besides, advices were given when necessary to ensure the construction workforce were familiar with relevant procedures, and to maintain good environmental performance on site. Regular toolbox talks on environmental issues were organised for the construction workforce by the contractors to ensure understanding and proper implementation of environmental protection and pollution control mitigation measures.

2.6.1 Landscape and Visual Mitigation Measures

Implementation of applicable landscape and visual mitigation measures (reference to the environmental protection measures CM1 – CM10 in **Appendix B**) is monitored regularly in accordance with the Manual. The implementation status of the environmental protection measures is summarised below in **Table 2.24**. For trees which were managed by the Project during the reporting period, relevant measures have been implemented by Contracts 3302, 3503, 3602 and 3801. Contracts 3508 and 3802 would begin to undertake tree management measures subject to the handover of site area (Contract 3508: Q2 2021 (tentative); Contract 3802: to be confirmed). Those trees which were within the Project boundary yet to be taken care by existing 3RS Contractors during the reporting period were managed by AAHK. It should be noted that the Hong Kong Observatory issued the No 3 and No 9 typhoon signals when Tropical Storm Sinlaku and Typhoon Higos hit Hong Kong on 1 and 18 August 2020 respectively during the reporting period. The third wave of COVID – 19 epidemics has impacted Hong Kong during the reporting period. A land parcel with trees were removed as the land was acquired by the government for construction of temporary emergency hospital to handle COVID-19 pandemic, and is now no longer under the management of the Project.

The total number of retained trees of the Project as of September 2020 was 147. Compared to 228 retained trees reported in the previous reporting quarter, the change in number was due to the following reasons:

- Trees near Airport North Interchange were collapsed due to adverse weather from the two typhoons mentioned above (-4 nos.);
- A land parcel with 14 retained trees was acquired by the government for construction of emergency hospital to handle COVID19 pandemic at AsiaWorld-Expo, and is no longer managed by the Project (-14 nos);

- Contractor's initial tree survey covered some areas which recently confirmed not to be works areas and therefore excluded from the Project area. Trees located in those areas were removed from the retained tree list under the Project (-72 nos.); and
- ET and contractor's recent on-site inspections confirmed that the status of 9 nos. of trees near the Airport North Interchange should be retained trees (+9 nos.).

The total number of transplanted trees of the Project as of September 2020 was 5 which is 3 number less when compared to previous reporting quarter. The reasons that the 3 transplanted trees were removed are listed below (also shown in **Table 2.26**):

- A tree near Chek Lap Kok South Road Interchange (establishment period completed and was in maintenance period) was collapsed due to adverse weather from Typhoon Higos (-1 no.); and
- A land parcel with 2 transplanted trees (establishment period completed and was in maintenance period) was acquired by the government for the construction of emergency hospital to handle COVID19 pandemic at AsiaWorld-Expo (AWE), and is no longer managed by the Project (-2 nos). The trees were later felled.

Details of the retained trees, transplanted trees and to-be-transplanted trees under the Project are summarized in

Table 2.25. Photos of retained, transplanted and to-be-transplanted trees are presented in **Appendix C**.

Table 2.24: Landscape and Visual – Construction Phase Audit Summary

Landscape and Visual Mitigation Measures during Construction Implementation Status	Implementation Status	Relevant Contract(s) in the Reporting Period
CM1- The construction area and contractor's temporary works areas shall be minimised to avoid impacts on adjacent landscape.	The implementation of mitigation measures were checked by ET during weekly site inspection and clarified by the Contractors during the monthly Environmental Management Meetings. Implementation of the measures CM5, CM6 and CM7 by Contractors was observed.	3RS Project contracts
CM2 – Reduction of construction period to practical minimum.		
CM3 – Phasing of the construction stage to reduce visual impacts during the construction phase.		
CM4 – Construction traffic (land and sea) including construction plants, construction vessels and barges shall be kept to a practical minimum.		
CM5 – Erection of decorative mesh screens or construction hoardings around works areas in visually unobtrusive colours.		
CM6 – Avoidance of excessive height and bulk of site buildings and structures		
CM7 – Control of night-time lighting by hooding all lights and through minimisation of night working periods		

Landscape and Visual Mitigation Measures during Construction Implementation Status	Implementation Status	Relevant Contract(s) in the Reporting Period
<p>CM8 – All existing trees shall be carefully protected during construction. Detailed Tree Protection Specification shall be provided in the Contract Specification. Under this specification, the Contractor shall be required to submit, for approval, a detailed working method statement for the protection of trees prior to undertaking any works adjacent to all retained trees, including trees in contractor's works areas</p>	<p>Tree Protection Specifications have been provided in the relevant Contract Specifications respectively for implementation by the Contractors under the Project.</p> <p>The Contractors' performance on the implementation of the trees maintenance and protection measures were observed and checked by the ET weekly during construction period.</p> <p>The cumulative total number of retained trees under the 3RS Project in the reporting period was updated to be 147.</p>	<p>3302, 3503, 3602, 3801</p> <p>3508 (To be implemented)</p>
<p>CM9 – Trees unavoidably affected by the works shall be transplanted where practical. A detailed Tree Transplanting Specification shall be provided in the Contract Specification, if applicable. Sufficient time for necessary tree root and crown preparation periods shall be allowed in the project programme</p>	<p>Tree Transplanting Specifications have been provided in the relevant Contract Specifications respectively for implementation by the Contractors under the Project where trees will unavoidably be affected by the construction works.</p> <p>The Contractors were required to submit Method Statements for tree transplanting prior to the transplanting works. Tree inspections were conducted by ET to check the tree transplanting works implemented by the Contractors on site. The cumulative total number of transplanted trees under the Project was five. Details of the transplanted trees are presented in Table 2.26.</p> <p>The Contractors' performance on the implementation of trees maintenance and protection measures on transplanted trees were observed and checked by the ET bi-monthly during the 12-month establishment period respectively.</p> <p>Long-term management of the transplanted trees were monitored by ET annually during the first 10 years after the establishment period.</p>	<p>3503, 3801</p> <p>3508 (To be implemented)</p>
<p>CM 10 – Land formation works shall be followed with advanced hydroseeding around taxiways and runways as soon as practical</p>	<p>To be implemented around taxiways and runways as soon as practicable.</p>	<p>To be implemented</p>

Table 2.25: Summary of the Number of Retained, Transplanted and To-be-transplanted Trees in the Reporting Period

Existing				
Contract	Retained (nos.)	Transplanted (nos.)		To-be-transplanted (nos.)
		Establishment Period	Maintenance Period	
3302	9	0	0	0
3503	19	3	0	6
3602	2	0	0	0
3801	117	0	5 ⁽¹⁾	0
Sub-total	147	3	5 ⁽¹⁾	6
Provisional				
Contract	Retained (nos.)	Transplanted (nos.)		To-be-transplanted (nos.)
3508 ⁽²⁾	155	0		22
Sub-total	155	0		22
Grand Total	302	5		28

Notes:

- (1) CT1253 and CT276 were handed over to Southern Landside Petrol Filling Station (SLPS) in Jun 2019. Another 3 transplanted trees (CT1194, CT1794 and CT1795) were subsequently fell after transplantation. Please refer **Table 2.26** for details.
- (2) Actual tree number is subject to confirmation after initial tree survey is conducted by the Contractor.

Table 2.26: Summary of the Transplanted Trees Updated in the Reporting Period

Tree ID	Transplant Date	Management Stage	Management Agency	Remarks
CT276	3 May 2018	<u>Establishment period</u> 4 May 2018 – May 2019	Contract 3801	NA
		<u>Maintenance period</u> Jun 2019 – May 2028	Southern Landside Petrol Filling Station	
CT1253	4 May 2018	<u>Establishment period</u> 5 May 2018 – May 2019	Contract 3801	
		<u>Maintenance period</u> Jun 2019 – May 2028	Southern Landside Petrol Filling Station	
T835	22 Jan 2020	<u>Establishment period</u> 23 Jan 2020 – Jan 2021	Contract 3503	NA
T836	13 Dec 2019	<u>Establishment period</u> 14 Dec 2020 – Jan 2021	Contract 3503	
T838	22 Jan 2020	<u>Establishment period</u> 23 Jan 2020 – Jan 2021	Contract 3503	
CT1194	4 May 2018	<u>Establishment period</u> 5 May 2018 – May 2019	Contract 3801	Uprooted and collapsed due to damage by Typhoon Higos on 18 Aug 2020. Tree removal was conducted as recommended by Contractor's tree specialist.
		<u>Maintenance period</u> Jun 2019 – May 2028	Southern Landside Petrol Filling Station	
CT1794	3 May 2018	<u>Establishment period</u> 4 May 2018 – May 2019	Contract 3801	The tree within the land parcel was acquired by the government for construction of emergency hospital to handle COVID19 pandemic at AWE. The tree was fell.
		<u>Maintenance period</u> Jun 2019 – May 2028	AWE	

Tree ID	Transplant Date	Management Stage	Management Agency	Remarks
CT1795	3 May 2018	<u>Establishment period</u> 4 May 2018 – May 2019	Contract 3801	The tree within the land parcel was acquired by the government for construction of emergency hospital to handle COVID19 pandemic at AWE. The tree was fell.
		<u>Maintenance period</u> Jun 2019 – May 2028	AWE	

2.6.2 Land Contamination Assessment

The Supplementary CAP was submitted to EPD pursuant to EP Condition 2.20. The CARs for Golf Course and T2 Emergency Power Supply System Nos.1 (Volumes 1 and 2), 2, 3, and 5 were submitted to EPD in accordance with EP Condition 1.9 and the Supplementary CAP in which no land contamination issues were identified.

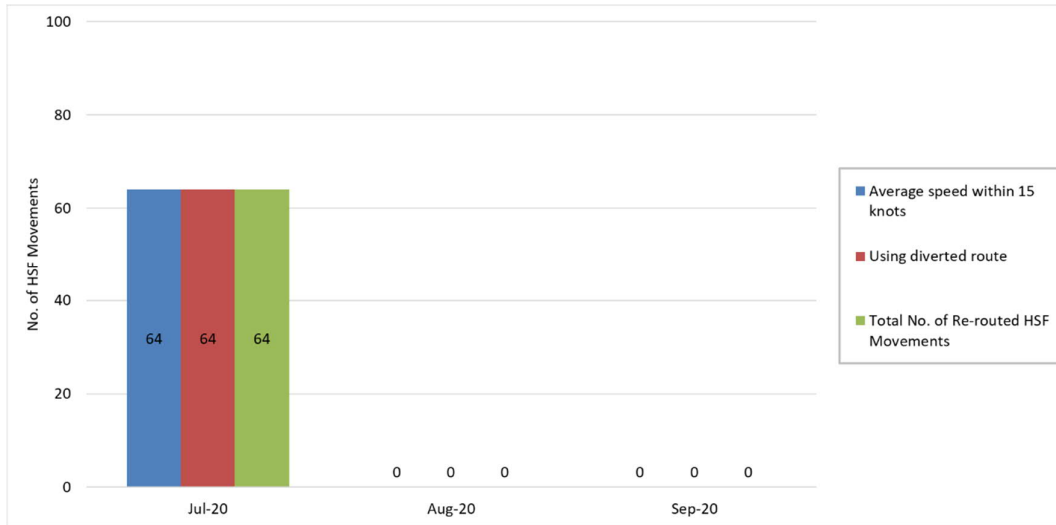
A summary of implementation status of the environmental mitigation measures for the construction phase of the Project during the reporting period is provided in **Appendix B**.

2.7 Audit of SkyPier High Speed Ferries

Due to the COVID-19 pandemic, all SkyPier HSF services have been suspended from 25 March 2020 until further notice. Special ferry service between Macau and Hong Kong International Airport was arranged from 17 June 2020 to 16 July 2020. In total, 64 ferry movements between HKIA SkyPier and Macau were audited in the reporting period. The daily movements of all SkyPier HSFs in the reporting period ranged between 0 and 4, which fell within the maximum daily cap number of 125.

The average speeds of all HSFs travelling through the Speed Control Zone (SCZ) ranged from 10.6 to 13.4 knots. All HSFs travelled through the SCZ with average speed within 15 knots in compliance with the Marine Travel Routes and Management Plan for High Speed Ferries of SkyPier (the SkyPier Plan). The summary of the SkyPier Plan monitoring result is presented in **Graph 1**.

Insufficient AIS data cases were received from some HSFs during the reporting period. After investigation, it was found that AIS data for the concerned ferries were missing due to signal interference as reported by the ferry operators after checking the condition of the AIS transponders. Vessel captains were requested to provide the radar track photos which indicated the vessels entered the SCZ through the gate access points and there was no speeding in the SCZ. Ferry operators' explanations were accepted.

Graph 1: Summary of SkyPier High Speed Ferries Monitoring Results

In addition, the dolphin habitat index was reviewed based on AFCD latest marine mammals monitoring report findings and historical dolphin density records, and the grids for dolphin hotspot remain unchanged. AAHK will continue to implement the SkyPier HSF route diversion and speed restriction according to the approved SkyPier Plan.

2.8 Audit of Construction and Associated Vessels

On the implementation of the updated Marine Travel Routes and Management Plan for Construction and Associated Vessels (MTRMP-CAV), the Maritime Surveillance System (MSS) automatically recorded deviation cases such as speeding, entering no entry zone, and not traveling through the designated gates. ET conducted bi-weekly audit of relevant information including AIS data, vessel tracks and other relevant records to ensure sufficient information were provided by the system and the contractors complied with the requirements of the MTRMP-CAV. The contractors submitted 3-month rolling vessel plans for construction vessel activities to AAHK in order to help maintain the number of construction vessels to a practicable minimum. The IEC also performed audit on the compliance of the requirements as part of the EM&A programme.

During the reporting period, deviations including speeding within the works area, entry from non-designated gates, and entering no-entry zones were identified. After investigation by the contractor's Marine Traffic Control Centre (MTCC) representatives, all the concerned captains were reminded to comply with the requirements of the MTRMP-CAV.

A total of 7 skipper training workshops were held by ET during the reporting period and 35 concerned captains of construction vessels associated with the 3RS contracts were trained to familiarise them with the predefined routes, general education on local cetaceans, guidelines for avoiding adverse water quality impact, the required environmental practices / measures while operating construction and associated vessels under the Project, and guidelines for operating vessels safely in the presence of CWDs. Another 22 skipper training workshops were held with 34 captains by contractors' Environmental Officers and competency tests were conducted subsequently with the trained captains by ET.

2.9 Review of the Key Assumptions Adopted in the EIA Report

With reference to Appendix E of the Manual, it is noted that the key assumptions adopted in approved EIA report for the construction phase are still valid and no major changes are involved.

The environmental mitigation measures recommended in the approved EIA Report remain applicable and shall be implemented in undertaking construction works for the Project.

3 Report on Non-compliance, Complaints, Notifications of Summons and Prosecutions

3.1 Compliance with Other Statutory Environmental Requirements

During the reporting period, environmental related licenses and permits required for the construction activities were checked. No non-compliance with environmental statutory requirements was recorded.

3.2 Analysis and Interpretation of Complaints, Notification of Summons and Status of Prosecutions

3.2.1 Complaints

Three environmental complaints were received in the reporting period. All environmental complaints were attended to and investigation was conducted by the ET in accordance with the Manual and the Complaint Management Plan. The summary of the complaints and analysis is presented

Table 3.1: Summary of Environmental Complaints

Date of Complaint Received	Details	Analysis/ Remedial Actions	Status
6 Jul 2020	A complaint was received regarding suspected improper chemical waste disposal at the pier near Marina Garden.	Investigation was conducted by the ET in accordance with the Manual and the Complaint Management Plan of the Project. According to the information provided by the Contactor, skipper of the concerned vessel carried a rubbish bin filled with trash from the vessel for disposal at the refuse collection point at the pier near Marina Garden, Tuen Mun. The chemical containers located at the pier near Marina Garden, Tuen Mun did not belong to the Contractor or the owner of the concerned vessel. Moreover, there was no chemical waste generated from the Contractor. Based on the ET's regular site inspections, the Contractor was mainly conducting ground investigation works and no chemical waste had been generated from the construction site. ET also checked the Contractor's chemical waste cabinet, which was found empty on 8 June, 3 and 6 July 2020. According to Contractor's record, no chemical waste was generated from the Contractor. The chemical waste cabinet was also observed properly locked and labelled with the display of the Chemical Waste Producer Registration on the cabinet, following the guideline Code of Practice of chemical waste handling. In view of the information provided by the Contractor and the ET's inspection findings, there were no evidences indicating improper disposal of chemical waste at the pier near Marine Garden, Tuen Mun by the Contractor. ET will continue to remind all work contracts to properly handle their chemical waste.	Closed

13 Jul 2020	A complaint was received regarding the discharge of muddy water from the construction site which was near Aviation Fuel Supply Company (AFSC) Operation Limited premise to the surrounding surface water channel.	Investigation was conducted by the ET in accordance with the Manual and the Complaint Management Plan of the Project. ET investigated the related work contracts of 3RS Project at or near the alleged area. Based on information provided by the Contractors, the sites were used as material stockpile and site office, and no wastewater was generated. Each contractor implemented water pollution control measures such as provision of concrete bund at the site entrance to contain surface runoff within the site, establishing an internal water reuse circuit using trenches and water tanks, and reuse of wastewater generated from the wheel washing facility. Based on the ET's weekly site inspections, no malpractices were observed on site. ET also found that surface runoff, if any, was contained within the Contractors respective site areas. In view of the above information provided from Contractors and ET inspection findings, there were no evidences that any one of the Contractors had discharged muddy water from their site areas to the surface water channel. However, the ET will continue to remind all work contracts to properly handle the wastewater, especially surface runoff during the rainy season.	Closed
28 Aug 2020	A complaint was received regarding dust issue at Chek Lap Kok South Road.	The case was investigated by ET in accordance with the Manual and the Complaint Management Plan of the Project. The anonymous complainant mentioned that there were two barges moored near Chek Lap Kok South Road and were causing dust nuisance to the surrounding area. The ET confirmed with EPD that the incident was occurred on 27 August 2020 morning and no further details of the barges / truck could be provided on this case. Southeast Quay is a public pier at Chek Lap Kok South Road which could be used by both 3RS and non-3RS contractors. The ET identified the potential related 3RS contractors based on AAHK's Quay Allocation Schedule and then conducted investigation on them. Based on information provided by the Contractors, dust mitigation measures including wetting of materials before unloading, and properly covering of trucks during transportation were in place. A full-time supervisor was also assigned on site by one of the Contractors to ensure environmental mitigation measures are properly implemented. Furthermore, the ET conducted an on-site investigation on 1 September 2020 morning and no malpractice nor dust impact was observed in the operation. It was noted that 3RS air monitoring results for August 2020 were within the corresponding Action and Limit Levels at all monitoring stations. Nevertheless, the ET will continue to remind the potential related contractors to strengthen their environmental mitigation measures for dust suppression.	Closed

3.2.2 Notifications of Summons or Status of Prosecution

No notification of summons nor prosecution was received during the reporting period.

3.3 Cumulative Statistics

Cumulative statistics on valid exceedance, non-compliance, complaints, notifications of summons and status of prosecutions are summarised in **Table 3.2** and **Table 3.3**.

Table 3.2: Statistics for Valid Exceedances for the Environmental Monitoring

		Total No. Recorded in the Reporting Period	Total No. Recorded since the Project Commenced
1-hr TSP	Action Level	0	0
	Limit Level	0	0

		Total No. Recorded in the Reporting Period	Total No. Recorded since the Project Commenced
Noise	Action Level	0	0
	Limit Level	0	0
Waste	Action Level	0	0
	Limit Level	0	0
Water	Action Level	0	0
	Limit Level	0	0
CWD	Action Level	0	0
	Limit Level	0	0

Remark: Non-project related triggers of Action or Limit Level are not shown in this table.

Table 3.3: Statistics for Non-compliance, Complaints, Notifications of Summons and Prosecution

Reporting Period	Cumulative Statistics			
	Non-compliance	Complaints	Notifications of Summons	Prosecutions
This reporting period	0	3	0	0
From 28 December 2015 to end of the reporting period	0	20	1	1

4 Conclusion and Recommendation

In this quarterly period from 1 July 2020 to 30 September 2020, the EM&A programme has been implemented as planned, including 96 sets of air quality measurements, 52 sets of construction noise measurements, 38 sets of water quality measurements, 6 complete sets of vessel line transect surveys and 6 days of land-based theodolite tracking survey effort for CWD monitoring, as well as environmental site inspections and waste monitoring for the Project's construction works.

The key activities of the Project carried out in the reporting period included reclamation works and land-based works. Works in the reclamation areas included DCM works, marine filling, seawall and facilities construction, together with runway and associated works. Land-based works on existing airport island involved mainly airfield works, foundation and substructure work for Terminal 2 expansion, modification and tunnel work for APM and BHS, and preparation work for utilities, with activities include site establishment, site office construction, road and drainage works, cable ducting, demolition, piling, and excavation works.

Monitoring results of construction dust, construction noise, construction waste, and CWD did not trigger the corresponding Action and Limit Levels in the reporting period. All site observations made by the ET were recorded in the site inspection checklists and passed to the contractor together with the recommended follow-up actions.

For water quality, the water quality monitoring results for all parameters, except DO, obtained during the reporting period were within the corresponding Action and Limit Levels stipulated in the EM&A programme. Relevant investigation and follow-up actions will be conducted according to the EM&A programme if the corresponding Action and Limit Levels are triggered. For DO and SS, some testing results triggered the relevant Action or Limit Levels, and the corresponding investigations were conducted accordingly. The investigation findings concluded that the cases were not related to the Project. In summary, the construction activities in the reporting period did not introduce adverse impact to all water quality sensitive receivers.

In total, 64 ferry movements between HKIA SkyPier and Macau audited in the reporting period. All HSFs travelled through the SCZ with average speed within 15 knots in compliance with the SkyPier Plan.

During the reporting period, ET conducted bi-weekly audit of the MSS to ensure the system recorded all deviation cases accurately and the contractors fully complied with the requirements of the MTRMP-CAV. 7 skipper training workshops were held by ET and 22 skipper training workshops were held by contractors' Environmental Officers during the reporting period and competency tests were conducted subsequently with the trained skippers by ET.

On the implementation of MMWP, dolphin observers were deployed by the contractors for laying of silt curtains for marine filling works in accordance with the plan. On the implementation of DEZ Plan, dolphin observers were deployed for continuous monitoring of the DEZ by the contractors for DCM works and seawall construction in accordance with the DEZ Plan. Trainings for the proposed dolphin observers were provided by the ET prior to the aforementioned works. From the contractors' MMWP observation records and DEZ monitoring records, no dolphin or other marine mammals were observed within or around the silt curtains or the DEZ in this reporting period. Audits of acoustic decoupling for construction vessels were also carried out by the ET.

The recommended environmental mitigation measures, as included in the EM&A programme, were effectively implemented during the reporting period. Also, the EM&A programme

implemented by the ET has effectively monitored the construction activities and ensured the proper implementation of mitigation measures.

Figures

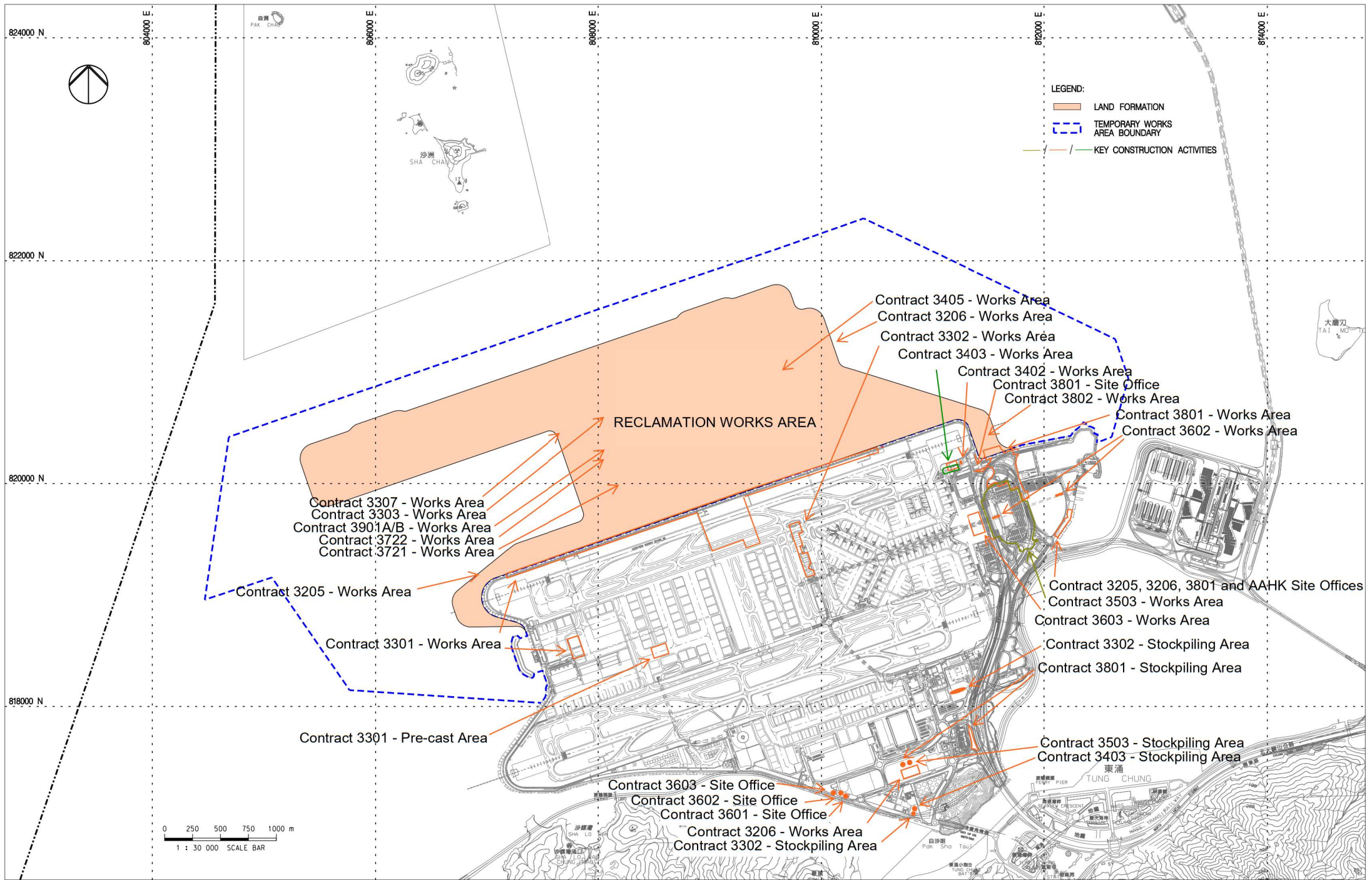


FIGURE 1.1 LOCATIONS OF KEY CONSTRUCTION ACTIVITIES

Note: The locations are for indicative purpose. The actual construction work locations are in accordance with the construction work programme.



80000 E.

80000 E.

81000 E.

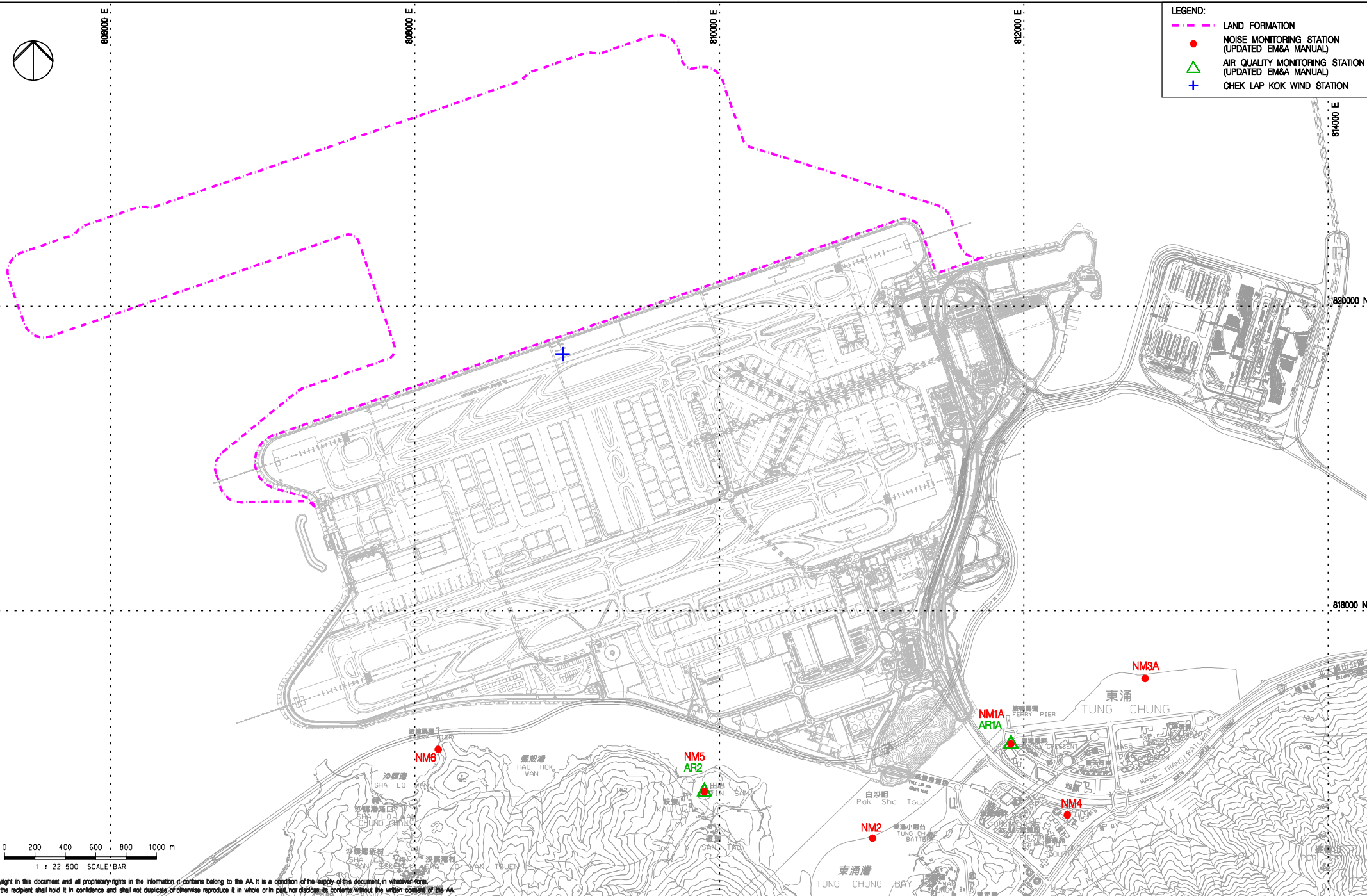
82000 E.

84000 E.

82000 N.

81800 N.

- LEGEND:
- LAND FORMATION
 - NOISE MONITORING STATION (UPDATED EM&A MANUAL)
 - AIR QUALITY MONITORING STATION (UPDATED EM&A MANUAL)
 - CHEK LAP KOK WIND STATION



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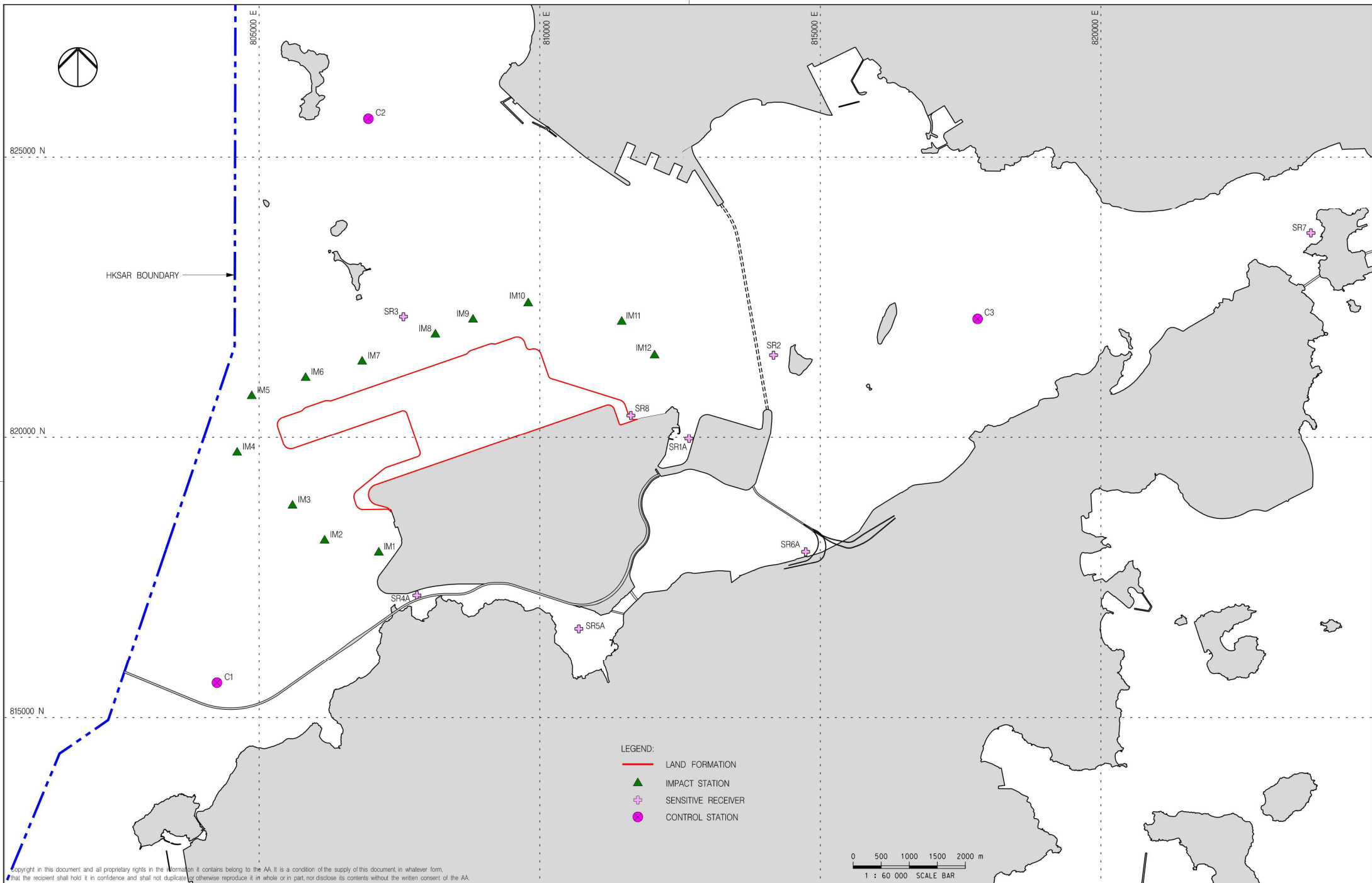
Rev.	Date	Description	Checked
A	06JAN16	FIRST ISSUE	RO
B	28JAN16	GENERAL REVISION	RO
C	11FEB16	GENERAL REVISION	RO
D	29OCT18	GENERAL REVISION	SH



Title
LOCATIONS OF AIR AND NOISE MONITORING STATIONS AND CHEK LAP KOK WIND STATION

Consultant's Signatures for Approval		Date
Design	TK	29OCT18
Checkers	TK	29OCT18
Approver	EC	29OCT18

EXPANSION OF HONG KONG INTERNATIONAL AIRPORT INTO A THREE-RUNWAY SYSTEM	
Drawing No.	Scale at A3 1:22500
FIGURE 2.1	Rev. D



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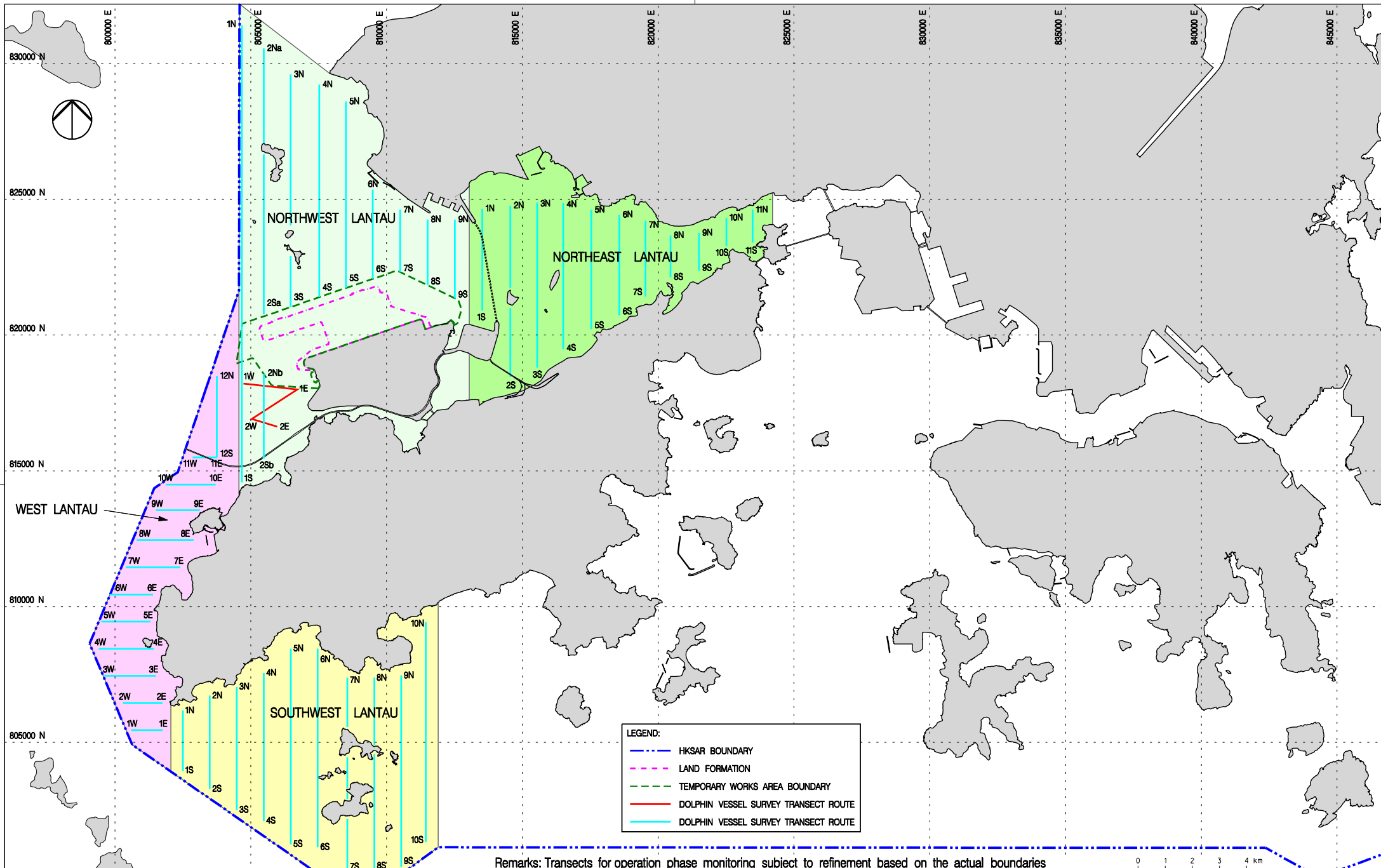
Rev.	Date	Description	Checked
A	21AUG19	FIRST ISSUE	VL



Title
WATER QUALITY MONITORING STATIONS

Consultant's Signatures for Approval		Date
Design	DC	21AUG19
Checkers	DC / TK	21AUG19
Approver	EC	21AUG19

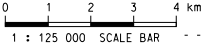
EXPANSION OF HONG KONG INTERNATIONAL AIRPORT INTO A THREE-RUNWAY SYSTEM	
Drawing No.	Scale at A3 1 : 60000
FIGURE 2.2	Rev. A



LEGEND:

- HKSAR BOUNDARY
- LAND FORMATION
- TEMPORARY WORKS AREA BOUNDARY
- DOLPHIN VESSEL SURVEY TRANSECT ROUTE
- DOLPHIN VESSEL SURVEY TRANSECT ROUTE

Remarks: Transects for operation phase monitoring subject to refinement based on the actual boundaries for the extension of Hong Kong International Airport Approach Areas (HKIAAA) and 3RS Marine Park



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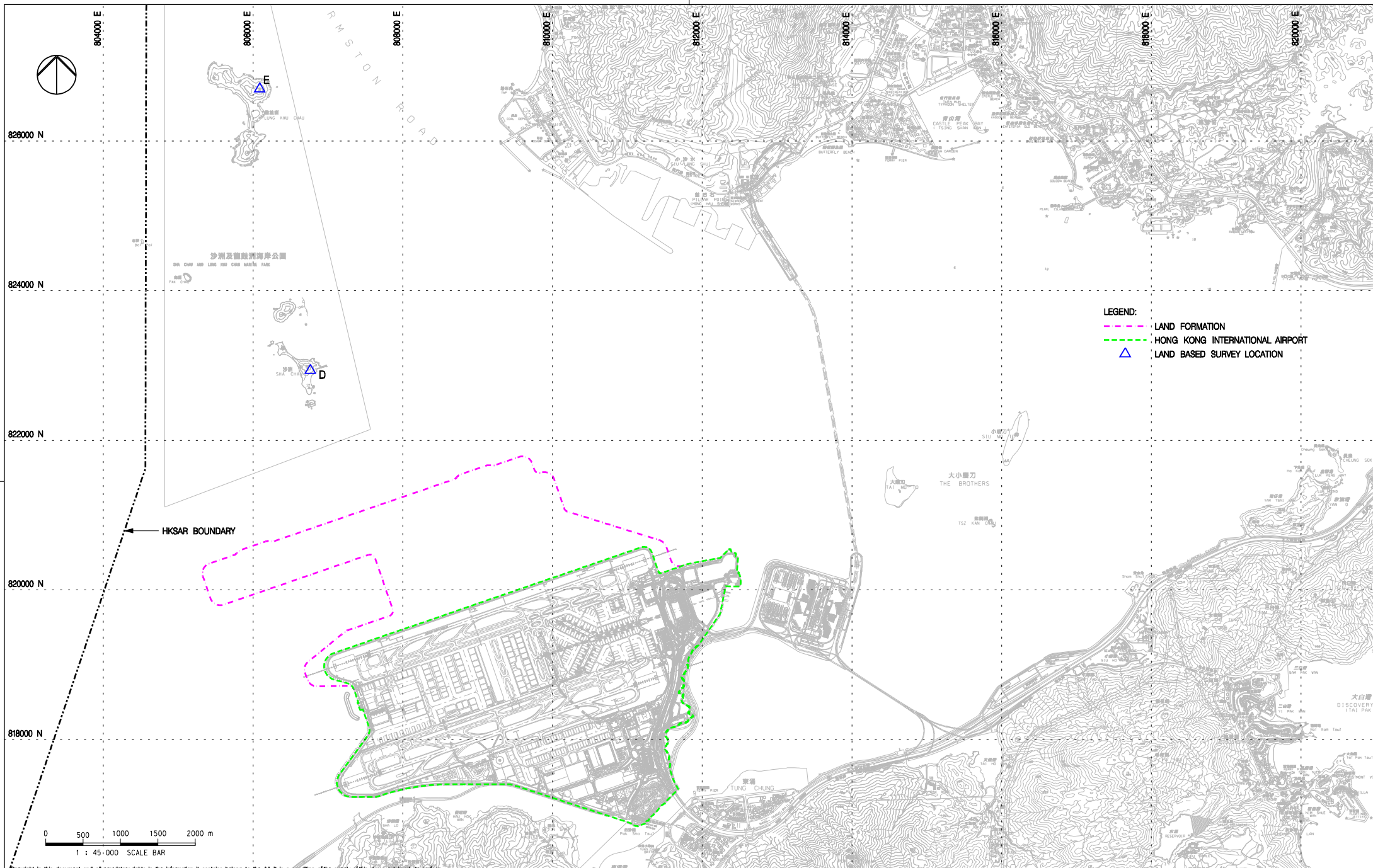
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B	27JUL16	GENERAL REVISION	JT
C	06FEB17	GENERAL REVISION	JT
D	01MAR17	GENERAL REVISION	JT
E	29OCT18	GENERAL REVISION	SH
F	04APR19	GENERAL REVISION	SH



Title
VESSEL BASED DOLPHIN MONITORING
TRANSECTS IN CONSTRUCTION,
POST-CONSTRUCTION AND OPERATION PHASES

Consultant's Signatures for Approval		Date
Design	JC	04APR19
Checkers	JC / TK	04APR19
Approver	EC	04APR19

EXPANSION OF HONG KONG INTERNATIONAL AIRPORT INTO A THREE-RUNWAY SYSTEM	
Drawing No.	Scale at A3 1 : 125000
FIGURE 2.3	
Rev.	F



LEGEND:
 --- LAND FORMATION
 --- HONG KONG INTERNATIONAL AIRPORT
 ▲ LAND BASED SURVEY LOCATION

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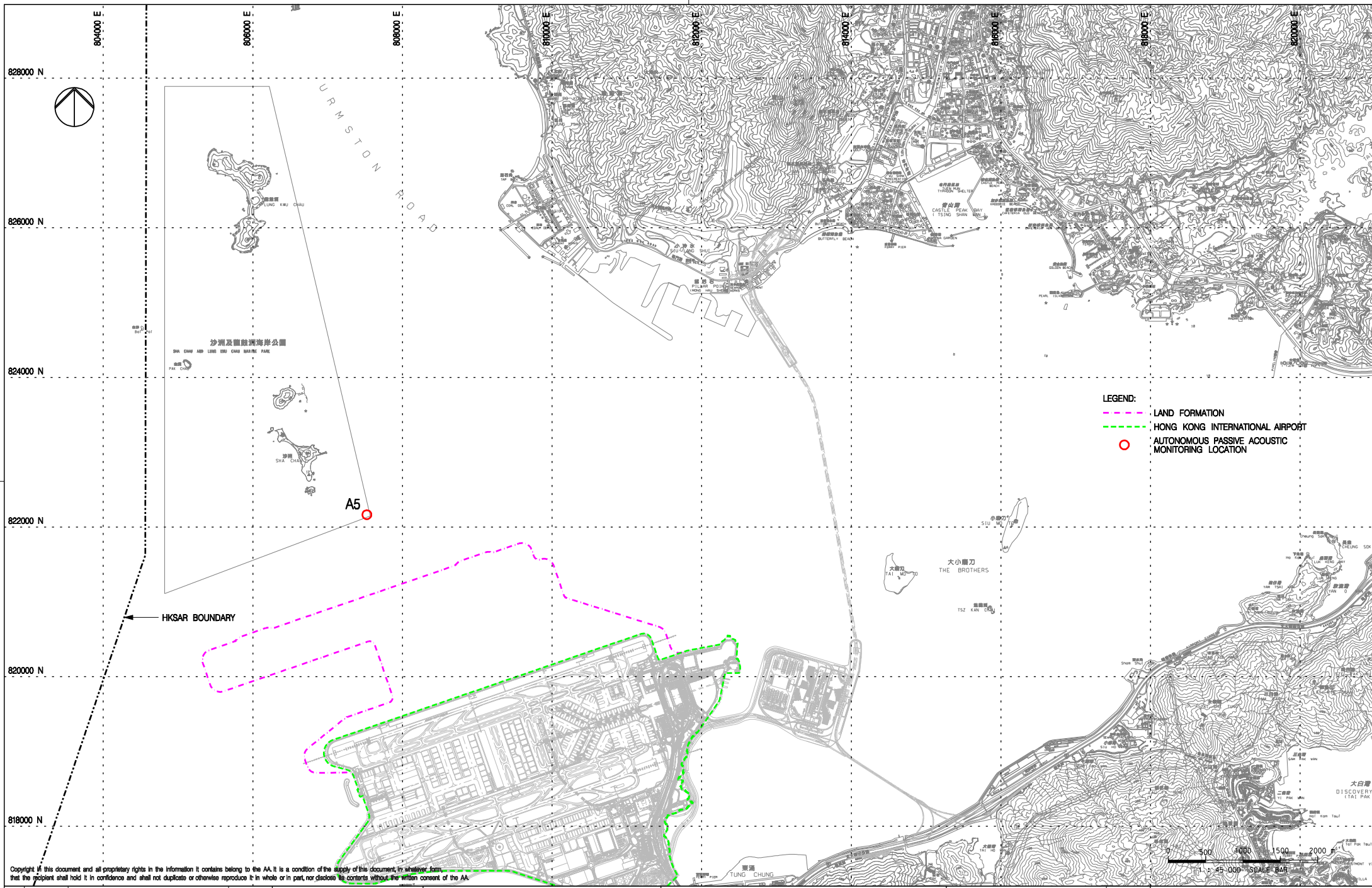
Rev.	Date	Description	Checked
A	02DEC15	FIRST ISSUE	JC
B	06FEB17	GENERAL REVISION	JC
C	29OCT18	GENERAL REVISION	SH



Title
 LAND BASED DOLPHIN MONITORING
 IN BASELINE AND CONSTRUCTION PHASES

Consultant's Signatures for Approval		Date
Design	JC	29OCT18
Checkers	JC / TK	29OCT18
Approver	EC	29OCT18

EXPANSION OF HONG KONG INTERNATIONAL AIRPORT INTO A THREE-RUNWAY SYSTEM	
Drawing No.	FIGURE 2.4
Scale at A3	1:45000
Rev.	C



LEGEND:
 - - - - - LAND FORMATION
 - - - - - HONG KONG INTERNATIONAL AIRPORT
 ○ AUTONOMOUS PASSIVE ACOUSTIC MONITORING LOCATION

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A	29AUG17	FIRST ISSUE	JT
B	10OCT17	GENERAL REVISION	PL
C	29OCT18	GENERAL REVISION	SH

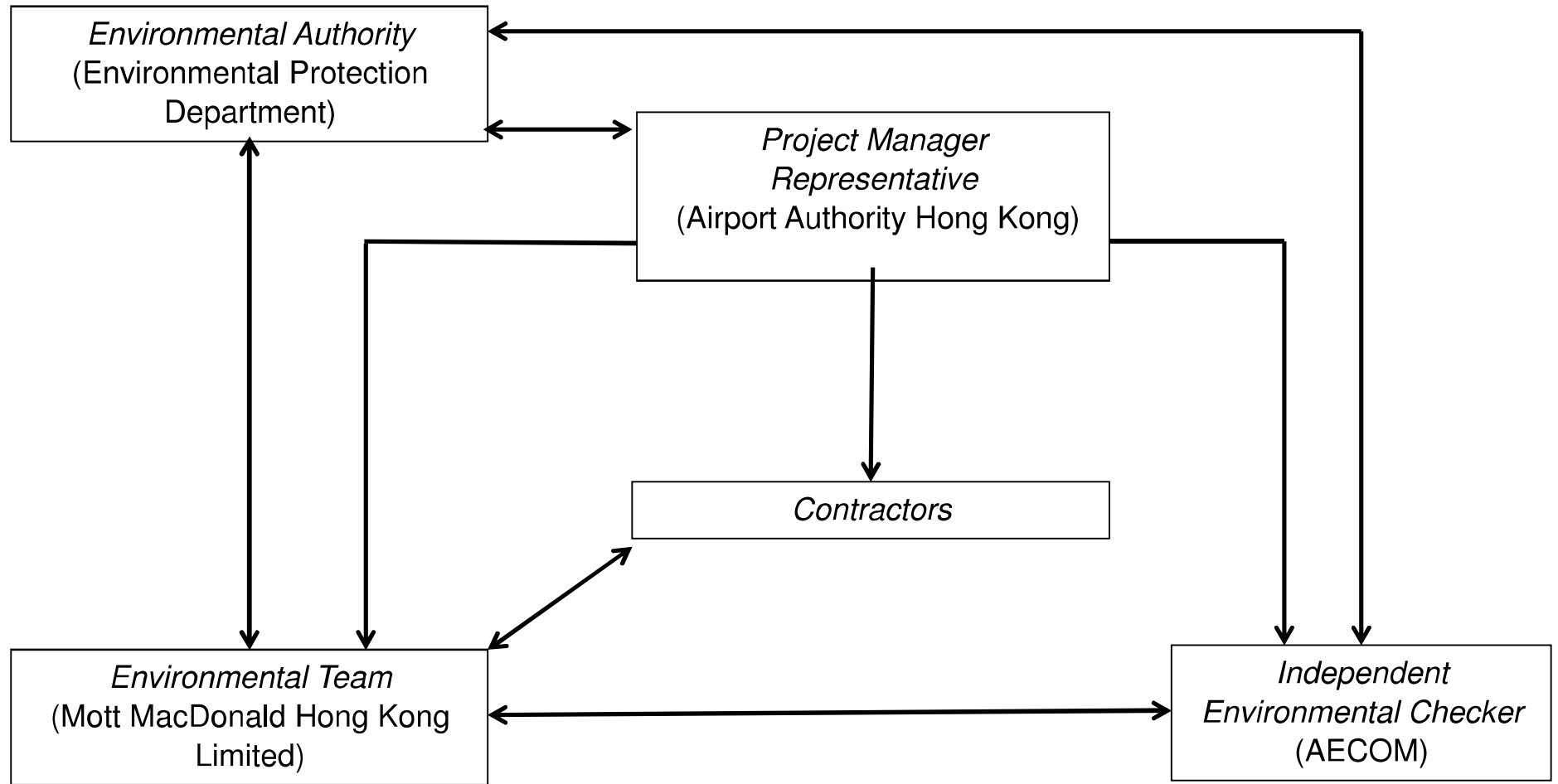


Title
 LOCATION FOR AUTONOMOUS PASSIVE ACOUSTIC MONITORING

Consultant's Signatures for Approval		Date
Design	JC	29OCT18
Checkers	JC / TK	29OCT18
Approver	EC	29OCT18

EXPANSION OF HONG KONG INTERNATIONAL AIRPORT INTO A THREE-RUNWAY SYSTEM
 Drawing No. **FIGURE 2.10**
 Scale at A3 **1:45000**
 Rev. **C**

Appendix A. Project Organization Chart



Appendix B. Environmental Mitigation Implementation Schedule (EMIS) for Construction Phase

Environmental Mitigation Implementation Schedule (EMIS) for Construction Phase

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
Air Quality Impact – Construction Phase					
5.2.6.2	2.1	-	Dust Control Measures <ul style="list-style-type: none"> Water spraying for 12 times a day or once every two hours for 24-hour working at all active works area. 	Within construction site / Duration of the construction phase	I
5.2.6.3	2.1	-	<ul style="list-style-type: none"> Covering of at least 80% of the stockpiling area by impervious sheets. Water spraying of all dusty materials immediately prior to any loading transfer operation so as to keep the dusty material wet during material handling. 	Within construction site / Duration of the construction phase	I
5.2.6.4	2.1	-	Dust control practices as stipulated in the Air Pollution Control (Construction Dust) Regulation should be adopted. These practices include: Good Site Management <ul style="list-style-type: none"> Good site management is important to help reducing potential air quality impact down to an acceptable level. As a general guide, the Contractor should maintain high standard of housekeeping to prevent emission of fugitive dust. Loading, unloading, handling and storage of raw materials, wastes or by-products should be carried out in a manner so as to minimise the release of visible dust emission. Any piles of materials accumulated on or around the work areas should be cleaned up regularly. Cleaning, repair and maintenance of all plant facilities within the work areas should be carried out in a manner minimising generation of fugitive dust emissions. The material should be handled properly to prevent fugitive dust emission before cleaning. 	Within construction site / Duration of the construction phase	I
			Disturbed Parts of the Roads <ul style="list-style-type: none"> Each and every main temporary access should be paved with concrete, bituminous hardcore materials or metal plates and kept clear of dusty materials; or Unpaved parts of the road should be sprayed with water or a dust suppression chemical so as to keep the entire road surface wet. 	Within construction site / Duration of the construction phase	I
			Exposed Earth <ul style="list-style-type: none"> Exposed earth should be properly treated by compaction, hydroseeding, vegetation planting or seating with latex, vinyl, bitumen within six months after the last construction activity on the site or part of the site where the exposed earth lies. 	Within construction site / Duration of the construction phase	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p>Loading, Unloading or Transfer of Dusty Materials</p> <ul style="list-style-type: none"> All dusty materials should be sprayed with water immediately prior to any loading or transfer operation so as to keep the dusty material wet. 	Within construction site / Duration of the construction phase	I
			<p>Debris Handling</p> <ul style="list-style-type: none"> Any debris should be covered entirely by impervious sheeting or stored in a debris collection area sheltered on the top and the three sides; and Before debris is dumped into a chute, water should be sprayed so that it remains wet when it is dumped. 	Within construction site / Duration of the construction phase	I
			<p>Transport of Dusty Materials</p> <ul style="list-style-type: none"> Vehicle used for transporting dusty materials/spoils should be covered with tarpaulin or similar material. The cover should extend over the edges of the sides and tailboards. 	Within construction site / Duration of the construction phase	I
			<p>Wheel washing</p> <ul style="list-style-type: none"> Vehicle wheel washing facilities should be provided at each construction site exit. Immediately before leaving the construction site, every vehicle should be washed to remove any dusty materials from its body and wheels. 	Within construction site / Duration of the construction phase	I
			<p>Use of vehicles</p> <ul style="list-style-type: none"> The speed of the trucks within the site should be controlled to about 10km/hour in order to reduce adverse dust impacts and secure the safe movement around the site; Immediately before leaving the construction site, every vehicle should be washed to remove any dusty materials from its body and wheels; and Where a vehicle leaving the construction site is carrying a load of dusty materials, the load should be covered entirely by clean impervious sheeting to ensure that the dusty materials do not leak from the vehicle. 	Within construction site / Duration of the construction phase	I
			<p>Site hoarding</p> <ul style="list-style-type: none"> Where a site boundary adjoins a road, street, service lane or other area accessible to the public, hoarding of not less than 2.4m high from ground level should be provided along the entire length of that portion of the site boundary except for a site entrance or exit. 	Within construction site / Duration of the construction phase	I
5.2.6.5	2.1	-	<p>Best Practices for Concrete Batching Plant</p> <p>The relevant best practices for dust control as stipulated in the Guidance Note on the Best Practicable Means for Cement Works (Concrete Batching Plant) BPM 3/2 as well as in the future Specified Process licence should be adopted. The best practices are recommended to be applied to both the land based and floating concrete batching plants. Best practices include:</p> <p>Cement and other dusty materials</p>	Within Concrete Batching Plant / Duration of the construction phase	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ The loading, unloading, handling, transfer or storage of cement, pulverised fuel ash (PFA) and/or other equally dusty materials shall be carried in a totally enclosed system acceptable to EPD. All dust-laden air or waste gas generated by the process operations shall be properly extracted and vented to fabric filtering system to meet the required emission limit; ▪ Cement, PFA and/or other equally dusty materials shall be stored in storage silo fitted with audible high level alarms to warn of over-filling. The high-level alarm indicators shall be interlocked with the material filling line such that in the event of the silo approaching an overfilling condition, an audible alarm will operate, and after 1 minute or less the material filling line will be closed; ▪ Vents of all silos shall be fitted with fabric filtering system to meet the required emission limit; ▪ Vents of cement/PFA weighing scale shall be fitted with fabric filtering system to meet the required emission limit; and ▪ Seating of pressure relief valves of all silos shall be checked, and the valves re-seated if necessary, before each delivery. 		
			<p>Other raw materials</p> <ul style="list-style-type: none"> ▪ The loading, unloading, handling, transfer or storage of other raw materials which may generate airborne dust emissions such as crushed rock, sand, stone aggregate, shall be carried out in such a manner to prevent or minimize dust emissions; ▪ The materials shall be adequately wetted prior to and during the loading, unloading and handling operations. Manual or automatic water spraying system shall be provided at all unloading areas, stock piles and material discharge points; ▪ All receiving hoppers for unloading relevant materials shall be enclosed on three sides up to 3 m above the unloading point. In no case shall these hoppers be used as the material storage devices; ▪ The belt conveyor for handling materials shall be enclosed on top and two sides with a metal board at the bottom to eliminate any dust emission due to wind-whipping effect. Other type of enclosure will also be accepted by EPD if it can be demonstrated that the proposed enclosure can achieve same performance; ▪ All conveyor transfer points shall be totally enclosed. Openings for the passage of conveyors shall be fitted with adequate flexible seals; ▪ Scrapers shall be provided at the turning points of all conveyors to remove dust adhered to the belt surface; ▪ Conveyors discharged to stockpiles of relevant materials shall be arranged to minimize free fall as far as practicable. All free falling transfer points from conveyors to stockpiles shall be enclosed with chute(s) and water sprayed; ▪ Aggregates with a nominal size less than or equal to 5 mm should be stored in totally enclosed structure such as storage bin and should not be handled in open area. Where there is sufficient buffer area surrounding the concrete batching plant, ground stockpiling may be used; 	<p>Within Concrete Batching Plant / Duration of the construction phase</p>	<p>N/A</p>

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ The stockpile shall be enclosed at least on top and three sides and with flexible curtain to cover the entrance side; ▪ Aggregates with a nominal size greater than 5 mm should preferably be stored in a totally enclosed structure. If open stockpiling is used, the stockpile shall be enclosed on three sides with the enclosure wall sufficiently higher than the top of the stockpile to prevent wind whipping; and ▪ The opening between the storage bin and weighing scale of the materials shall be fully enclosed. 		
			<p>Loading of materials for batching</p> <ul style="list-style-type: none"> ▪ Concrete truck shall be loaded in such a way as to minimise airborne dust emissions. The following control measures shall be implemented: <ul style="list-style-type: none"> (a) Pre-mixing the materials in a totally enclosed concrete mixer before loading the materials into the concrete truck is recommended. All dust-laden air generated by the pre-mixing process as well as the loading process shall be totally vented to fabric filtering system to meet the required emission limit; and (b) If truck mixing batching or other types of batching method is used, effective dust control measures acceptable to EPD shall be adopted. The dust control measures must have been demonstrated to EPD that they are capable to collect and vent all dust-laden air generated by the material loading/mixing to dust arrestment plant to meet the required emission limit. ▪ The loading bay shall be totally enclosed during the loading process. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Vehicles</p> <ul style="list-style-type: none"> ▪ All practicable measures shall be taken to prevent or minimize the dust emission caused by vehicle movement; and ▪ All access and route roads within the premises shall be paved and adequately wetted. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Housekeeping</p> <ul style="list-style-type: none"> ▪ A high standard of housekeeping shall be maintained. All spillages or deposits of materials on ground, support structures or roofs shall be cleaned up promptly by a cleaning method acceptable to EPD. Any dumping of materials at open area shall be prohibited. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
5.2.6.6	2.1	-	<p>Best Practices for Asphaltic Concrete Plant</p> <p>The relevant best practices for dust control as stipulated in the Guidance Note on the Best Practicable Means for Tar and Bitumen Works (Asphaltic Concrete Plant) BPM 15 (94) as well as in the future Specified Process licence should be adopted. These include:</p> <p>Design of Chimney</p> <ul style="list-style-type: none"> ▪ The chimney shall not be less than 3 metres plus the building height or 8 metres above ground level, whichever is the greater; ▪ The efflux velocity of gases from the main chimney shall not be less than 12 m/s at full load condition; 	Within Concrete Batching Plant / Duration of the construction phase	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ The flue gas exit temperature shall not be less than the acid dew point; and ▪ Release of the chimney shall be directed vertically upwards and not be restricted or deflected. 		
			<p>Cold feed side</p> <ul style="list-style-type: none"> ▪ The aggregates with a nominal size less than or equal to 5 mm shall be stored in totally enclosed structure such as storage bin and shall not be handled in open area; ▪ Where there is sufficient buffer area surrounding the plant, ground stockpiling may be used. The stockpile shall be enclosed at least on top and three sides and with flexible curtain to cover the entrance side. If these aggregates are stored above the feeding hopper, they shall be enclosed at least on top and three sides and be wetted on the surface to prevent wind-whipping; ▪ The aggregates with a nominal size greater than 5 mm should preferably be stored in totally enclosed structure. Aggregates stockpile that is above the feeding hopper shall be enclosed at least on top and three sides. If open stockpiling is used, the stockpiles shall be enclosed on three sides with the enclosure wall sufficiently higher than the top of the stockpile to prevent wind whipping; ▪ Belt conveyors shall be enclosed on top and two sides and provided with a metal board at the bottom to eliminate any dust emission due to the wind-whipping effect. Other type of enclosure will also be accepted by EPD if it can be demonstrated that the proposed enclosure can be achieve the same performance; ▪ Scrapers shall be provided at the turning points of all belt conveyors inside the chute of the transfer points to remove dust adhered to the belt surface; ▪ All conveyor transfer points shall be totally enclosed. Openings for the passages of conveyors shall be fitted with adequate flexible seals; and ▪ All materials returned from dust collection system shall be transferred in enclosed system and shall be stored inside bins or enclosures. 	<p>Within Concrete Batching Plant / Duration of the construction phase</p>	N/A
			<p>Hot feed side</p> <ul style="list-style-type: none"> ▪ The inlet and outlet of the rotary dryer shall be enclosed and ducted to a dust extraction and collection system such as a fabric filter. The particulate and gaseous concentration at the exhaust outlet of the dust collector shall not exceed the required limiting values; ▪ The bucket elevator shall be totally enclosed and the air be extracted and ducted to a dust collection system to meet the required particulates limiting value; ▪ All vibratory screens shall be totally enclosed and dust tight with close-fitted access inspection opening. Gaskets shall be installed to seal off any cracks and edges of any inspection openings; ▪ Chutes for carrying hot material shall be rigid and preferably fitted with abrasion resistant plate inside. They shall be inspected daily for leakages; 	<p>Within Concrete Batching Plant / Duration of the construction phase</p>	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ All hot bins shall be totally enclosed and dust tight with close-fitted access inspection opening. Gaskets shall be installed to seal off any cracks and edges of any inspection openings. The air shall be extracted and ducted to a dust collection system to meet the required particulates limiting value; and ▪ Appropriate control measures shall be adopted in order to meet the required bitumen emission limit as well as the ambient odour level (2 odour units). 		
			<p>Material transportation</p> <ul style="list-style-type: none"> ▪ The loading, unloading, handling, transfer or storage of other raw materials which may generate airborne dust emissions such as crushed rocks, sands, stone aggregates, reject fines, shall be carried out in such a manner as to minimize dust emissions; ▪ Roadways from the entrance of the plant to the product loading points and/or any other working areas where there are regular movements of vehicles shall be paved or hard surfaced; and ▪ Haul roads inside the Works shall be adequately wetted with water and/or chemical suppressants by water trucks or water sprayers. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Control of emissions from bitumen decanting</p> <ul style="list-style-type: none"> ▪ The heating temperature of the particular bitumen type and grade shall not exceed the corresponding temperature limit of the same type listed in Appendix 1 of the Guidance Note; ▪ Tamper-free high temperature cut-off device shall be provided to shut off the fuel supply or electricity in case the upper limit for bitumen temperature is reached; ▪ Proper chimney for the discharge of bitumen fumes shall be provided at high level; ▪ The emission of bitumen fumes shall not exceed the required emission limit; and <p>The air-to-fuel ratio shall be properly controlled to allow complete combustion of the fuel. The fuel burners, if any, shall be maintained properly and free from carbon deposits in the burner nozzles.</p>	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Liquid fuel</p> <ul style="list-style-type: none"> ▪ The receipt, handling and storage of liquid fuel shall be carried out so as to prevent the release of emissions of organic vapours and/or other noxious and offensive emissions to the air. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Housekeeping</p> <ul style="list-style-type: none"> ▪ A high standard of housekeeping shall be maintained. Waste material, spillage and scattered piles gathered beneath belt conveyors, inside and around enclosures shall be cleared frequently. The minimum clearing frequency is on a weekly basis. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
5.2.6.7	2.1	-	<p>Best Practices for Rock Crushing Plants</p> <p>The relevant best practices for dust control as stipulated in the Guidance Note on the Best Practicable Means for Mineral Works (Stone Crushing Plant) BPM 11/1 (95) as well as in the future Specified Process licence should be adopted. These include:</p>	Within Concrete Batching Plant / Duration of the construction phase	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p>Crushers</p> <ul style="list-style-type: none"> ▪ The outlet of all primary crushers, and both inlet and outlet of all secondary and tertiary crushers, if not installed inside a reasonably dust tight housing, shall be enclosed and ducted to a dust extraction and collection system such as a fabric filter; ▪ The inlet hopper of the primary crushers shall be enclosed on top and 3 sides to contain the emissions during dumping of rocks from trucks. The rock while still on the trucks shall be wetted before dumping; ▪ Water sprayers shall be installed and operated in strategic locations at the feeding inlet of crushers; and ▪ Crusher enclosures shall be rigid and be fitted with self-closing doors and close-fitting entrances and exits. Where conveyors pass through the crusher enclosures, flexible covers shall be installed at entries and exits of the conveyors to the enclosure. 		
			<p>Vibratory screens and grizzlies</p> <ul style="list-style-type: none"> ▪ All vibratory screens shall be totally enclosed in a housing. Screenhouses shall be rigid and reasonably dust tight with self-closing doors or close-fitted entrances and exits for access. Where conveyors pass through the screenhouse, flexible covers shall be installed at entries and exits of the conveyors to the housing. Where containment of dust within the screenhouse structure is not successful then a dust extraction and collection system shall be provided; and ▪ All grizzlies shall be enclosed on top and 3 sides and sufficient water sprayers shall be installed at their feeding and outlet areas. 	Within Concrete Batching Plant / Duration of the construction phase	N/A
			<p>Belt conveyors</p> <ul style="list-style-type: none"> ▪ Except for those conveyors which are placed within a totally enclosed structure such as a screenhouse or those erected at the ground level, all conveyors shall be totally enclosed with windshield on top and 2 sides; ▪ Effective belt scraper such as the pre-cleaner blades made by hard wearing materials and provided with pneumatic tensioner, or equivalent device, shall be installed at the head pulley of designated conveyor as required to dislodge fine dust particles that may adhere to the belt surface and to reduce carry-back of fine materials on the return belt. Bottom plates shall also be provided for the conveyor unless it has been demonstrated that the corresponding belt scraper is effective and well maintained to prevent falling material from the return belt; and ▪ Except for those transfer points which are placed within a totally enclosed structure such as a screenhouse, all transfer points to and from conveyors shall be enclosed. Where containment of dust within the enclosure is not successful, then water sprayers shall be provided. Openings for any enclosed structure for the passage of conveyors shall be fitted with flexible seals. 	Within Concrete Batching Plant / Duration of the construction phase	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p>Storage piles and bins</p> <ul style="list-style-type: none"> Where practicable, free falling transfer points from conveyors to stockpiles shall be fitted with flexible curtains or be enclosed with chutes designed to minimize the drop height. Water sprays shall also be used where required. The surface of all surge piles and stockpiles of blasted rocks or aggregates shall be kept sufficiently wet by water spraying wherever practicable; All open stockpiles for aggregates of size in excess of 5 mm shall be kept sufficiently wet by water spraying where practicable; or The stockpiles of aggregates 5 mm in size or less shall be enclosed on 3 sides or suitably located to minimize wind-whipping. Save for fluctuations in stock or production, the average stockpile shall stay within the enclosure walls and in no case the height of the stockpile shall exceed twice the height of the enclosure walls. Scattered piles gathered beneath belt conveyors, inside and around enclosures shall be cleared regularly. 	<p>Within Concrete Batching Plant / Duration of the construction phase</p>	N/A
			<p>Rock drilling equipment</p> <ul style="list-style-type: none"> Appropriate dust control equipment such as a dust extraction and collection system shall be used during rock drilling activities. 	<p>Within Concrete Batching Plant / Duration of the construction phase</p>	N/A
Hazard to Human Life – Construction Phase					
Table 6.40	3.2	-	<ul style="list-style-type: none"> Precautionary measures should be established to request barges to move away during typhoons. 	<p>Construction Site / Construction Period</p>	
Table 6.40	3.2	-	<ul style="list-style-type: none"> An appropriate marine traffic management system should be established to minimize risk of ship collision. 	<p>Construction Site / Construction Period</p>	
Table 6.40	3.2	-	<ul style="list-style-type: none"> Location of all existing hydrant networks should be clearly identified prior to any construction works. 	<p>Construction Site / Construction Period</p>	
Noise Impact – Construction Phase					
7.5.6	4.3	-	<p>Good Site Practice</p> <p>Good site practice and noise management can significantly reduce the impact of construction site activities on nearby NSRs. The following package of measures should be followed during each phase of construction:</p> <ul style="list-style-type: none"> only well-maintained plant to be operated on-site and plant should be serviced regularly during the construction works; machines and plant that may be in intermittent use to be shut down between work periods or should be throttled down to a minimum; 	<p>Within the Project site / During construction phase / Prior to commencement of operation</p>	

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> plant known to emit noise strongly in one direction, should, where possible, be orientated to direct noise away from the NSRs; mobile plant should be sited as far away from NSRs as possible; and material stockpiles and other structures to be effectively utilised, where practicable, to screen noise from on-site construction activities. 		
7.5.6	4.3	-	Adoption of QPME <ul style="list-style-type: none"> QPME should be adopted as far as applicable. 	Within the Project site / During construction phase / Prior to commencement of operation	I
7.5.6	4.3	-	Use of Movable Noise Barriers <ul style="list-style-type: none"> Movable noise barriers should be placed along the active works area and mobile plants to block the direct line of sight between PME and the NSRs. 	Within the Project site / During construction phase / Prior to commencement of operation	I
7.5.6	4.3	-	Use of Noise Enclosure/ Acoustic Shed <ul style="list-style-type: none"> Noise enclosure or acoustic shed should be used to cover stationary PME such as air compressor and generator. 	Within the Project site / During construction phase / Prior to commencement of operation	I
Water Quality Impact – Construction Phase					

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
8.8.1.2 and 8.8.1.3	5.1	2.26	<p>Marine Construction Activities</p> <p><u>General Measures to be Applied to All Works Areas</u></p> <ul style="list-style-type: none"> ▪ Barges or hoppers shall not be filled to a level which will cause overflow of materials or pollution of water during loading or transportation; ▪ Use of Lean Material Overboard (LMOB) systems shall be prohibited; ▪ Excess materials shall be cleaned from the decks and exposed fittings of barges and hopper dredgers before the vessels are moved; ▪ Plants should not be operated with leaking pipes and any pipe leakages shall be repaired quickly; ▪ Adequate freeboard shall be maintained on barges to reduce the likelihood of decks being washed by wave action; ▪ All vessels shall be sized such that adequate clearance is maintained between vessels and the sea bed at all states of the tide to ensure that undue turbidity is not generated by turbulence from vessel movement or propeller wash; ▪ The works shall not cause foam, oil, grease, litter or other objectionable matter to be present in the water within and adjacent to the works site; and ▪ For ground improvement activities including DCM, the wash water from cleaning of the drilling shaft should be appropriately treated before discharge. The Contractor should ensure the waste water meets the WPCO/TM requirements before discharge. No direct discharge of contaminated water is permitted. 	Within construction site / Duration of the construction phase	I
			<p><u>Specific Measures to be Applied to All Works Areas</u></p> <ul style="list-style-type: none"> ▪ The daily maximum production rates shall not exceed those assumed in the water quality assessment in the EIA report; ▪ A maximum of 10 % fines content to be adopted for sand blanket and 20 % fines content for marine filling below +2.5 mPD prior to substantial completion of seawall (until end of Year 2017) shall be specified in the works contract document; 	Within construction site / Duration of the construction phase	I
			<ul style="list-style-type: none"> ▪ An advance seawall of at least 200m to be constructed (comprising either rows of contiguous permanent steel cells completed above high tide mark or partially completed seawalls with rock core to high tide mark and filter layer on the inner side) prior to commencement of marine filling activities; ▪ Closed grab dredger shall be used to excavate marine sediment; ▪ Silt curtains surrounding the closed grab dredger shall be deployed in accordance with the Silt Curtain Deployment Plan; and 		<p>N/A</p> <p>*(The arrangement of silt curtain has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ The Silt Curtain Deployment Plan shall be implemented. 		I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p><u>Specific Measures to be Applied to Land Formation Activities prior to Commencement of Marine Filling Works</u></p> <ul style="list-style-type: none"> ▪ Double layer 'Type III' silt curtains to be applied around the active eastern works areas prior to commencement of sand blanket laying activities. The silt curtains shall be configured to minimise SS release during ebb tides. A silt curtain efficiency test shall be conducted to validate the performance of the silt curtains; 	<p>Within construction site / Duration of the construction phase</p>	<p>N/A</p> <p>*(The arrangement of silt curtain has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ Double layer silt curtains to enclose WSRs C7a and silt screens installed at the intake points for both WSR C7a and C8 prior to commencement of construction; and 		<p>For C7a, I</p> <p>For C8, I</p> <p>*(The requirement of silt curtain / screen has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ The silt curtains and silt screens should be regularly checked and maintained. 		<p>I</p>
			<p><u>Specific Measures to be Applied to Land Formation Activities during Marine Filling Works</u></p> <ul style="list-style-type: none"> ▪ Double layer 'Type II' or 'Type III' silt curtains to be applied around the eastern openings between partially completed seawalls prior to commencement of marine filling activities. The silt curtains shall be configured to minimise SS release during ebb tides; 	<p>Within construction site / Duration of the construction phase</p>	<p>I</p> <p>*(The arrangement of silt curtain has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ Double layer silt curtains to be applied at the south-western opening prior to commencement of marine filling activities; 		<p>N/A</p> <p>*(The arrangement of silt curtain has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ Double layer silt curtain to enclose WSR C7a and silt screens installed at the intake points for both WSR C7a and C8 prior to commencement of marine filling activities; and 		<p>N/A</p> <p>*(The requirement of silt curtain / screen has been modified. The details can be referred to Silt Curtain Deployment Plan)</p>
			<ul style="list-style-type: none"> ▪ The silt curtains and silt screens should be regularly checked and maintained. 		<p>I</p>

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p><u>Specific Measures to be Applied to the Field Joint Excavation Works for the Submarine Cable Diversion</u></p> <ul style="list-style-type: none"> Only closed grabs designed and maintained to avoid spillage shall be used and should seal tightly when operated. Excavated materials shall be disposed at designated marine disposal area in accordance with the Dumping at Sea Ordinance (DASO) permit conditions; and Silt curtains surrounding the closed grab dredger to be deployed as a precautionary measure. 	Within construction site / Duration of the construction phase	N/A
8.8.1.4	5.1	-	<p>Modification of the Existing Seawall</p> <ul style="list-style-type: none"> Silt curtains shall be deployed around the seawall modification activities to completely enclose the active works areas, and care should be taken to avoid splashing of rockfill / rock armour into the surrounding marine environment. For the connecting sections with the existing outfalls, works for these connection areas should be undertaken during the dry season in order that individual drainage culvert cells may be isolated for interconnection works. 	At the existing northern seawall / Duration of the construction phase	N/A
8.8.1.5	5.1	-	<p>Construction of New Stormwater Outfalls and Modifications to Existing Outfalls</p> <ul style="list-style-type: none"> During operation of the temporary drainage channel, runoff control measures such as bunding or silt fence shall be provided on both sides of the channel to prevent accumulation and release of SS via the temporary channel. Measures should also be taken to minimise the ingress of site drainage into the culvert excavations. 	Within construction site / Duration of the construction phase	N/A
8.8.1.6 8.8.1.7	5.1	2.27	<p>Piling Activities for Construction of New Runway Approach Lights and HKIAAA Marker Beacons</p> <p>Silt curtains shall be deployed around the piling activities to completely enclose the piling works and care should be taken to avoid spillage of excavated materials into the surrounding marine environment.</p> <p><u>For construction of the eastern approach lights at the CMPs</u></p> <ul style="list-style-type: none"> Ground improvement via DCM using a close-spaced layout shall be completed prior to commencement of piling works; Steel casings shall be installed to enclose the excavation area prior to commencement of excavation; The excavated materials shall be removed using a closed grab within the steel casings; No discharge of the cement mixed materials into the marine environment will be allowed; and Excavated materials shall be treated and reused on-site. 	Within construction site / Duration of the construction phase	N/A
8.8.1.8	5.1	-	<p>Construction of Site Runoff and Drainage</p> <p>The site practices outlined in ProPECC Note PN 1/94 should be followed as far as practicable in order to minimise surface runoff and the chance of erosion. The following measures are recommended:</p> <ul style="list-style-type: none"> Install perimeter cut-off drains to direct off-site water around the site and implement internal drainage, erosion and sedimentation control facilities. Channels, earth bunds or sand bag barriers should be provided on site to direct storm water to silt removal facilities. The design of the temporary on-site 	Within construction site / Duration of the construction phase	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p>drainage system should be undertaken by the Contractors prior to the commencement of construction (for works areas located on the existing Airport island) or as soon as the new land is completed (for works areas located on the new landform);</p>		
			<ul style="list-style-type: none"> ▪ Sand/silt removal facilities such as sand/silt traps and sediment basins should be provided to remove sand/silt particles from runoff to meet the requirements of the TM-DSS standards under the WPCO. The design of efficient silt removal facilities should make reference to the guidelines in Appendix A1 of ProPECC Note PN 1/94. Sizes may vary depending upon the flow rate. The detailed design of the sand/silt traps should be undertaken by the Contractors prior to the commencement of construction; 		I
			<ul style="list-style-type: none"> ▪ All drainage facilities and erosion and sediment control structures should be regularly inspected and maintained to ensure proper and efficient operation at all times and particularly during rainstorms. Deposited silt and grit should be regularly removed, at the onset of and after each rainstorm to ensure that these facilities are functioning properly; 		I
			<ul style="list-style-type: none"> ▪ Measures should be taken to minimize the ingress of site drainage into excavations. If excavation of trenches in wet periods is necessary, they should be dug and backfilled in short sections wherever practicable. Water pumped out from foundation excavations should be discharged into storm drains via silt removal facilities; 		I
			<ul style="list-style-type: none"> ▪ In the event that contaminated groundwater is identified at excavation areas, this should be treated on-site using a suitable wastewater treatment process. The effluent should be treated according to the requirements of the TM-DSS standards under the WPCO prior to discharge to foul sewers or collected for proper disposal off-site. No direct discharge of contaminated groundwater is permitted; and 		N/A
			<ul style="list-style-type: none"> ▪ All vehicles and plant should be cleaned before leaving a construction site to ensure no earth, mud, debris and the like is deposited by them on roads. An adequately designed and sited wheel washing facility should be provided at construction site exits. Wash-water should have sand and silt settled out and removed regularly to ensure the continued efficiency of the process. The section of access road leading to, and exiting from, the wheel-wash bay to the public road should be paved with sufficient backfall toward the wheel-wash bay to prevent vehicle tracking of soil and silty water to public roads and drains. All washwater should be treated according to the requirements of the TM-DSS standards under the WPCO prior to discharge. 		I
8.8.1.9	5.1	-	<p>Sewage Effluent from Construction Workforce</p> <ul style="list-style-type: none"> ▪ Temporary sanitary facilities, such as portable chemical toilets, should be employed on-site where necessary to handle sewage from the workforce. A licensed contractor should be employed to provide appropriate and adequate portable toilets and be responsible for appropriate disposal and maintenance. 	Within construction site / During construction phase	I
8.8.1.10	5.1		<p>General Construction Activities</p> <ul style="list-style-type: none"> ▪ Construction solid waste, debris and refuse generated on-site should be collected, handled and disposed of properly to avoid entering any nearby storm water drain. Stockpiles of cement and other construction materials should be kept covered when not being used; and 	Within construction site / During construction phase	I
8.8.1.11					

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
8.8.1.12 8.8.1.13	5.1	2.28	<ul style="list-style-type: none"> ▪ Oils and fuels should only be stored in designated areas which have pollution prevention facilities. To prevent spillage of fuels and solvents to any nearby storm water drain, all fuel tanks and storage areas should be provided with locks and be sited on sealed areas, within bunds of a capacity equal to 110% of the storage capacity of the largest tank. The bund should be drained of rainwater after a rain event. <p>Drilling Activities for the Submarine Aviation Fuel Pipelines</p> <p>To prevent potential water quality impacts at Sha Chau, the following measures shall be applied:</p> <ul style="list-style-type: none"> ▪ A 'zero-discharge' policy shall be applied for all activities to be conducted at Sha Chau; ▪ No bulk storage of chemicals shall be permitted; and ▪ A containment pit shall be constructed around the drill holes. This containment pit shall be lined with impermeable lining and bunded on the outside to prevent inflow from off-site areas. 	Within construction site / During construction phase	I
			<p>At the airport island side of the drilling works, the following measures shall be applied for treatment of wastewater:</p> <ul style="list-style-type: none"> ▪ During pipe cleaning, appropriate desilting or sedimentation device should be provided on site for treatment before discharge. The Contractor should ensure discharge water from the sedimentation tank meet the WPCO/TM requirements before discharge; and ▪ Drilling fluid used in drilling activities should be reconditioned and reused as far as possible. Temporary enclosed storage locations should be provided on-site for any unused chemicals that needs to be transported away after all the related construction activities are completed. The requirements in ProPECC Note PN 1/94 should be adhered to in the handling and disposal of bentonite slurries. 	Within construction site / During construction phase	I
Waste Management Implication – Construction Phase					
10.5.1.1	7.1	-	<p>Opportunities to minimise waste generation and maximise the reuse of waste materials generated by the project have been incorporated where possible into the planning, design and construction stages, and the following measures have been recommended:</p> <ul style="list-style-type: none"> ▪ The relevant construction methods (particularly for the tunnel works) and construction programme have been carefully planned and developed to minimise the extent of excavation and to maximise the on-site reuse of inert C&D materials generated by the project as far as practicable. Temporary stockpiling areas will also be provided to facilitate on-site reuse of inert C&D materials; ▪ Priority should be given to collect and reuse suitable inert C&D materials generated from other concurrent projects and the Government's PFRF as fill materials for the proposed land formation works; ▪ Only non-dredged ground improvement methods should be adopted in order to completely avoid the need for dredging and disposal of marine sediment for the proposed land formation work; ▪ Excavation work for constructing the APM tunnels, BHS tunnels and airside tunnels will not be down to the CMPs beneath the fill materials in order to avoid excavating any sediments; and 	Project Site Area / During design and construction phase	I
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EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ For the marine sediments expected to be excavated from the piling works of TRC, APM & BHS tunnels, airside tunnels and other facilities on the proposed land formation area, piling work of marine sections of the approach lights and HKIAAA beacons, basement works for some of T2 expansion area and excavation works for the proposed APM depot should be treated and reused on-site as backfilling materials, although required treatment level / detail and the specific re-use mode are under development. 		I
10.5.1.1	7.1	-	<p>The following good site practices should be performed during the construction activities include:</p> <ul style="list-style-type: none"> ▪ Nomination of an approved person, such as a site manager, to be responsible for good site practices, arrangements for collection and effective disposal to an appropriate facility, of all wastes generated at the site; ▪ Training of site personnel in proper waste management and chemical waste handling procedures; ▪ Provision of sufficient waste disposal points and regular collection for disposal; ▪ Appropriate measures to minimise windblown litter and dust during transportation of waste by either covering trucks by tarpaulin/ similar material or by transporting wastes in enclosed containers. The cover should be extended over the edges of the sides and tailboards; ▪ Stockpiles of C&D materials should be kept wet or covered by impervious sheets to avoid wind-blown dust; ▪ All dusty materials including C&D materials should be sprayed with water immediately prior to any loading transfer operation so as to keep the dusty material wet during material handling at the barging points/ stockpile areas; ▪ C&D materials to be delivered to and from the project site by barges or by trucks should be kept wet or covered to avoid wind-blown dust; ▪ The speed of the trucks including dump trucks carrying C&D or waste materials within the site should be controlled to about 10 km/hour in order to reduce the adverse dust impact and secure the safe movement around the site; and ▪ To avoid or minimise dust emission during transport of C&D or waste materials within the site, each and every main temporary access should be paved with concrete, bituminous hardcore materials or metal plates and kept clear of dusty materials. Unpaved parts of the road should be sprayed with water or a dust suppression chemical so as to keep the entire road surface wet. 	Project Site Area / Construction Phase	I
10.5.1.3	7.1	-	<p>The following practices should be performed to achieve waste reduction include:</p> <ul style="list-style-type: none"> ▪ Use of steel or aluminium formworks and falseworks for temporary works as far as practicable; ▪ Adoption of repetitive design to allow reuse of formworks as far as practicable; ▪ Segregation and storage of different types of waste in different containers, skips or stockpiles to enhance reuse or recycling of materials and their proper disposal; 	Project Site Area / Construction Phase	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> Encourage collection of aluminium cans, PET bottles and paper by providing separate labelled bins to enable these wastes to be segregated from other general refuse generated by the work force; Any unused chemicals or those with remaining functional capacity should be collected for reused as far as practicable; Proper storage and site practices to minimise the potential for damage or contamination of construction materials; and Plan and stock construction materials carefully to minimise amount of waste generated and avoid unnecessary generation of waste. 		
10.5.1.5	7.1		<ul style="list-style-type: none"> Inert and non-inert C&D materials should be handled and stored separately to avoid mixing the two types of materials. 	Project Site Area / Construction Phase	I
10.5.1.5	7.1	-	<ul style="list-style-type: none"> Any recyclable materials should be segregated from the non-inert C&D materials for collection by reputable licensed recyclers whereas the non-recyclable waste materials should be disposed of at the designated landfill site by a reputable licensed waste collector. 	Project Site Area / Construction Phase	I
10.5.1.6	7.1	-	<ul style="list-style-type: none"> A trip-ticket system promulgated shall be developed in order to monitor the off-site delivery of surplus inert C&D materials that could not be reused on-site for the proposed land formation work at the PFRF and to control fly tipping. 	Project Site Area / Construction Phase	I
10.5.1.6	7.1	2.32	<ul style="list-style-type: none"> The Contractor should prepare and implement a Waste Management Plan detailing various waste arising and waste management practices. 	Construction Phase	I
10.5.1.16	7.1	-	<p>The following mitigation measures are recommended during excavation and treatment of the sediments:</p> <ul style="list-style-type: none"> On-site remediation should be carried out in an enclosed area in order to minimise odour/dust emissions; The loading, unloading, handling, transfer or storage of treated and untreated sediment should be carried out in such a manner to prevent or minimise dust emissions; All practical measures, including but not limited to speed control for vehicles, should be taken to minimise dust emission; Good housekeeping should be maintained at all times at the sediment treatment facility and storage area; Treated and untreated sediment should be clearly separated and stored separately; and Surface runoff from the enclosed area should be properly collected and stored separately, and then properly treated to levels in compliance with the relevant effluent standards as required by the Water Pollution Control Ordinance before final discharge. 	Project Site Area / Construction Phase	I I I I I
10.5.1.18	7.1	-	<p>The marine sediments to be removed from the cable field joint area would be disposed of at the designated disposal sites to be allocated by the MFC. The following mitigation measures should be strictly</p>	Project Site Area / Construction Phase	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<p>followed to minimise potential impacts on water quality during transportation of the sediments requiring Type 1 disposal:</p> <ul style="list-style-type: none"> Bottom opening of barges shall be fitted with tight fitting seals to prevent leakage of material; Monitoring of the barge loading shall be conducted to ensure that loss of material does not take place during transportation. Transport barges or vessels shall be equipped with automatic self-monitoring devices as specified by EPD; and Barges or hopper barges shall not be filled to a level that would cause the overflow of materials or sediment laden water during loading or transportation. 		
10.5.1.19	7.1	-	<p>Contractor should register with the EPD as a chemical waste producer and to follow the relevant guidelines. The following measures should be implemented:</p> <ul style="list-style-type: none"> Good quality containers compatible with the chemical wastes should be used; Incompatible chemicals should be stored separately; Appropriate labels must be securely attached on each chemical waste container indicating the corresponding chemical characteristics of the chemical waste, such as explosive, flammable, oxidizing, irritant, toxic, harmful, corrosive, etc.; and The contractor will use a licensed collector to transport and dispose of the chemical wastes at the approved Chemical Waste Treatment Centre or other licensed recycling facilities, in accordance with the Waste Disposal (Chemical Waste) (General) Regulation. 	Project Site Area / Construction Phase	I
10.5.1.20	7.1	-	<ul style="list-style-type: none"> General refuse should be stored in enclosed bins or compaction units separated from inert C&D material. A reputable waste collector should be employed by the contractor to remove general refuse from the site for disposal at designated landfill sites. An enclosed and covered area should be provided to reduce the occurrence of 'wind blown' light material. 	Project Site Area / Construction Phase	I
10.5.1.21	7.1	-	<ul style="list-style-type: none"> The construction contractors will be required to regularly check and clean any refuse trapped or accumulated along the newly constructed seawall. Such refuse will then be stored and disposed of together with the general refuse. 	Project Site Area / Construction Phase	I
Land Contamination – Construction Phase					
11.10.1.2 to 11.10.1.3	8.1	2.32	<p>For areas inaccessible during site reconnaissance survey</p> <ul style="list-style-type: none"> Further site reconnaissance would be conducted once the areas are accessible in order to identify any land contamination concern for the areas. 	Project Site Area inaccessible during site reconnaissance / Prior to Construction Phase	I
			<ul style="list-style-type: none"> Subject to further site reconnaissance findings, a supplementary Contamination Assessment Plan (CAP) for additional site investigation (SI) (if necessary) may be prepared and submitted to EPD for endorsement prior to the commencement of SI at these areas. 		I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> ▪ After completion of SI, the Contamination Assessment Report (CAR) will be prepared and submitted to EPD for approval prior to start of the proposed construction works at the golf course, the underground and above-ground fuel storage tank areas, emergency power generation units, airside petrol filling station and fuel tank room. <hr/> <ul style="list-style-type: none"> ▪ Should remediation be required, Remediation Action Plan (RAP) and Remediation Report (RR) will be prepared for EPD's approval prior to commencement of the proposed remediation and any construction works respectively. 		<p>I *(CAR for golf course and Terminal 2 Emergency Power Supply System Nos.1, 2, 3, 4 and 5)</p> <hr/> <p>N/A</p>
11.8.1.2	8.1	-	<p>If contaminated soil is identified, the following mitigation measures are for the excavation and transportation of contaminated materials (if any):</p> <ul style="list-style-type: none"> ▪ To minimize the incidents of construction workers coming in contact with any contaminated materials, bulk earth-moving excavation equipment should be employed; ▪ Contact with contaminated materials can be minimised by wearing appropriate clothing and personal protective equipment such as gloves and masks (especially when working directly with contaminated material), provision of washing facilities and prohibition of smoking and eating on site; ▪ Stockpiling of contaminated excavated materials on site should be avoided as far as possible; ▪ The use of any contaminated soil for landscaping purpose should be avoided unless pre-treatment was carried out; ▪ Vehicles containing any excavated materials should be suitably covered to reduce dust emissions and/or release of contaminated wastewater; ▪ Truck bodies and tailgates should be sealed to prevent any discharge; ▪ Only licensed waste haulers should be used to collect and transport contaminated material to treatment/disposal site and should be equipped with tracking system to avoid fly tipping; ▪ Speed control for trucks carrying contaminated materials should be exercised. 8km/h is the recommended speed limit; ▪ Strictly observe all relevant regulations in relation to waste handling, such as Waste Disposal Ordinance (Cap 354), Waste Disposal (Chemical Waste) (General) Regulation (Cap 354) and obtain all necessary permits where required; and ▪ Maintain records of waste generation and disposal quantities and disposal arrangements. 	Project Site Area / Construction Phase	N/A
Terrestrial Ecological – Construction Phase					
12.10.1.1	9.2	2.14	<p>Pre-construction Egretty Survey</p> <ul style="list-style-type: none"> ▪ Conduct ecological survey for Sha Chau egretty to update the latest boundary of the egretty. 	Breeding season (April - July) prior to commencement of	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
				HDD drilling works at HKIA	
12.7.2.3 and 12.7.2.6	9.1	2.30	Avoidance and Minimisation of Direct Impact to Egret <ul style="list-style-type: none"> The daylighting location will avoid direct encroachment to the Sheung Sha Chau egret. The daylighting location and mooring of flat top barge, if required, will be kept away from the egret; In any event, controls such as demarcation of construction site boundary and confining the lighting within the site will be practised to minimise disturbance to off-site habitat at Sheung Sha Chau Island; and The containment pit at the daylighting location shall be covered or camouflaged. 	During construction phase at Sheung Sha Chau Island	
12.7.2.5	9.1	2.30	Preservation of Nesting Vegetation <ul style="list-style-type: none"> The proposed daylighting location and the arrangement of connecting pipeline will avoid the need of tree cutting, therefore the trees that are used by ardeids for nesting will be preserved. 	During construction phase at Sheung Sha Chau Island	
12.7.2.4 and 12.7.2.6	9.1	2.30	Timing the Pipe Connection Works outside Ardeid's Breeding Season <ul style="list-style-type: none"> All HDD and related construction works on Sheung Sha Chau Island will be scheduled outside the ardeids' breeding season (between April and July). No night-time construction work will be allowed on Sheung Sha Chau Island during all seasons. 	During construction phase at Sheung Sha Chau Island	
12.10.1.1	9.3	-	Ecological Monitoring <ul style="list-style-type: none"> During the HDD construction works period from August to March, ecological monitoring will be undertaken monthly at the HDD daylighting location on Sheung Sha Chau Island to identify and evaluate any impacts with appropriate actions taken as required to address and minimise any adverse impact found. 	at Sheung Sha Chau Island	
Marine Ecological Impact – Pre-construction Phase					
13.11.4.1	10.2.2	-	<ul style="list-style-type: none"> Pre-construction phase Coral Dive Survey. 	HKIAAA artificial seawall	
Marine Ecological Impact – Construction Phase					
13.11.1.3 to 13.11.1.6	-	-	Minimisation of Land Formation Area <ul style="list-style-type: none"> Minimise the overall size of the land formation needed for the additional facilities to minimise the overall loss of habitat for marine resources, especially the CWD population. 	Land formation footprint / during detailed design phase to completion of construction	
13.11.1.7 to 13.11.1.10	-	2.31	Use of Construction Methods with Minimal Risk/Disturbance <ul style="list-style-type: none"> Use of non-dredge method for the main land formation and ancillary works including the diversion of the aviation fuel pipeline to the AFRF; 	During construction phase at marine works area	

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> Use of Deep Cement Mixing (DCM) method instead of conventional seabed dredging for the land formation works to reduce the risk of negative impacts through the elevation of suspended solids and contaminants on CWDs, fisheries and the marine environment; 		I
			<ul style="list-style-type: none"> Use of bored piling in short duration to form the new approach lights and marker beacons for the new runway; 		N/A
			<ul style="list-style-type: none"> Avoid bored piling during CWD peak calving season (Mar to Jun); 		I
			<ul style="list-style-type: none"> Prohibition of underwater percussive piling; and 		I
			<ul style="list-style-type: none"> Use of horizontal directional drilling (HDD) method and water jetting methods for placement of submarine cables and pipelines to minimise the disturbance to the CWDs and other marine ecological resources. 		I
13.11.2.1 to 13.11.2.7	-	-	<p>Mitigation for Indirect Disturbance due to Deterioration of Water Quality</p> <ul style="list-style-type: none"> Water quality mitigation measures during construction phases include consideration of alternative construction methods, deployment of silt curtain and good site practices; Alternative construction methods including use of non-dredge methods for ground improvement (e.g. Deep Cement Mixing (DCM), prefabricated vertical drains (PVD), sand compaction piles, steel cells, stone columns and vertical sand drains); Use of bored piling in short duration to form the new approach lights and marker beacons for the new runway; and <p>Use of horizontal directional drilling (HDD) method and water jetting methods for placement of undersea cables and pipelines to minimise the disturbance to the CWDs and other marine ecological resources.</p>	All works area during the construction phase	I
					I
					N/A
					I
13.11.1.12	-	-	<p>Strict Enforcement of No-Dumping Policy</p> <ul style="list-style-type: none"> A policy prohibiting dumping of wastes, chemicals, oil, trash, plastic, or any other substance that would potentially be harmful to dolphins and/or their habitat in the work area; Mandatory educational programme of the no-dumping policy be made available to all construction site personnel for all project-related works; Fines for infractions should be implemented; and Unscheduled, on-site audits shall be implemented. 	All works area during the construction phase	I
13.11.1.13	-	-	<p>Good Construction Site Practices</p> <ul style="list-style-type: none"> Regular inspection of the integrity and effectiveness of all silt curtains and monitoring of effluents to ensure that any discharge meets effluent discharge guidelines; Keep the number of working or stationary vessels present on-site to the minimum anytime; and Unscheduled, on-site audits for all good site practice restrictions should be conducted, and fines or penalties sufficient to be an effective deterrent need to be levied against violators. 	All works area during the construction phase	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
13.11.1.3 to 13.11.1.6	-	-	Minimisation of Land Formation Area <ul style="list-style-type: none"> Minimise the overall size of the land formation needed for the additional facilities to minimise the overall loss of habitat for marine resources, especially the CWD population. 	Land formation footprint / during detailed design phase to completion of construction	I
13.11.5.4 to 13.11.5.13	10.3.1	-	SkyPier High Speed Ferries' Speed Restrictions and Route Diversions <ul style="list-style-type: none"> SkyPier HSFs operating to / from Zhuhai and Macau would divert north of SCLKC Marine Park with a 15 knot speed limit to apply for the part-journeys that cross high CWD abundance grid squares as indicatively shown in Drawing No. MCL/P132/EIA/13-023 of the EIA Report. Both the alignment of the northerly route and the portion of routings to be subject to the speed limit of 15 knots shall be finalised prior to commencement of construction based on the future review of up-to-date CWD abundance and EM&A data and taking reference to changes in total SkyPier HSF numbers; and A maximum of 10 knots will be enforced through the designated SCLKC Marine Park area at all times. Other mitigation measures <ul style="list-style-type: none"> The ET will audit various parameters including actual daily numbers of HSFs, compliance with the 15-knot speed limit in the speed control zone and diversion compliance for SkyPier HSFs operating to / from Zhuhai and Macau; and The effectiveness of the CWD mitigation measures after implementation of initial six month SkyPier HSF diversion and speed restriction will be reviewed. 	Area between the footprint and SCLKC Marine Park during construction phase	I
13.11.5.14 to 13.11.5.18	10.3.1	2.31	Dolphin Exclusion Zone <ul style="list-style-type: none"> Establishment of a 24 hr Dolphin Exclusion Zone (DEZ) with a 250 m radius around the land formation works areas; A DEZ would also be implemented during ground improvement works (e.g. DCM), water jetting works for submarine cables diversion, open trench dredging at the field joint locations and seawall construction; and A DEZ would also be implemented during bored piling work but as a precautionary measure only. 	Marine waters around land formation works area during construction phase	I
13.11.5.19	10.4	2.31	Acoustic Decoupling of Construction Equipment <ul style="list-style-type: none"> Air compressors and other noisy equipment that must be mounted on steel barges should be acoustically-decoupled to the greatest extent feasible, for instance by using rubber or air-filled tyres; and Specific acoustic decoupling measures shall be specified during the detailed design of the project for use during the land formation works. 	Around coastal works area during construction phase	I
13.11.5.20	10.6.1	2.29	Spill Response Plan	Construction phase	I

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
			<ul style="list-style-type: none"> An oil and hazardous chemical spill response plan is proposed to be established during the construction phase as a precautionary measure so that appropriate actions to prevent or reduce risks to CWDs can be undertaken in the event of an accidental spillage. 		
13.11.5.21 to 13.11.5.23	10.6.1	-	<p>Construction Vessel Speed Limits and Skipper Training</p> <ul style="list-style-type: none"> A speed limit of 10 knots should be strictly observed for construction vessels at areas with the highest CWD densities; and Vessels traversing through the work areas should be required to use predefined and regular routes (which would presumably become known to resident dolphins) to reduce disturbance to cetaceans due to vessel movements. Specific marine routes shall be specified by the Contractor prior to construction commencing. 	All areas north and west of Lantau Island during construction phase	
Fisheries Impact – Construction Phase					
14.9.1.2 to 14.9.1.5	-	-	<p>Minimisation of Land Formation Area</p> <ul style="list-style-type: none"> Minimise the overall size of the land formation needed for the additional facilities to minimise the overall loss of habitat for fisheries resources. 	Land formation footprint / during detailed design phase to completion of construction	
14.9.1.6	-	-	<p>Use of Construction Methods with Minimal Risk/Disturbance</p> <ul style="list-style-type: none"> Use of non-dredge method for the main land formation and ancillary works including the diversion of the aviation fuel pipeline to the AFRF; 	During construction phase at marine works area	
			<ul style="list-style-type: none"> Use of Deep Cement Mixing (DCM) method instead of conventional seabed dredging for the land formation works to reduce the risk of negative impacts through the elevation of suspended solids and contaminants on fisheries and the marine environment; 		
			<ul style="list-style-type: none"> Use of bored piling in short duration to form the new approach lights and marker beacons for the new runway; and 		N/A
			<ul style="list-style-type: none"> Use of horizontal directional drilling (HDD) method and water jetting methods for placement of undersea cables and pipelines to minimise the disturbance to fisheries resources. 		
14.9.1.11	-	-	<p>Strict Enforcement of No-Dumping Policy</p> <ul style="list-style-type: none"> A policy prohibiting dumping of wastes, chemicals, oil, trash, plastic, or any other substance that would potentially be harmful to dolphins and/or their habitat in the work area; Mandatory educational programme of the no-dumping policy be made available to all construction site personnel for all project-related works; Fines for infractions should be implemented; and Unscheduled, on-site audits shall be implemented. 	All works area during the construction phase	

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
14.9.1.12	-		Good Construction Site Practices <ul style="list-style-type: none"> Regular inspection of the integrity and effectiveness of all silt curtains and monitoring of effluents to ensure that any discharge meets effluent discharge guidelines; Keep the number of working or stationary vessels present on-site to the minimum anytime; and Unscheduled, on-site audits for all good site practice restrictions should be conducted, and fines or penalties sufficient to be an effective deterrent need to be levied against violators. 	All works area during the construction phase	
14.9.1.13 to 14.9.1.18	-		Mitigation for Indirect Disturbance due to Deterioration of Water Quality <ul style="list-style-type: none"> Water quality mitigation measures during construction phases include consideration of alternative construction methods, deployment of silt curtain and good site practices; Alternative construction methods including use of non-dredge methods for ground improvement (e.g. Deep Cement Mixing (DCM), prefabricated vertical drains (PVD), sand compaction piles, steel cells, stone columns and vertical sand drains); Use of bored piling in short duration to form the new approach lights and marker beacons for the new runway; and Use of horizontal directional drilling (HDD) method and water jetting methods for placement of undersea cables and pipelines to minimise the disturbance to fisheries resources. 	All works area during the construction phase	
Landscape and Visual Impact – Construction Phase					
Table 15.6	12.3	-	CM1 - The construction area and contractor’s temporary works areas should be minimised to avoid impacts on adjacent landscape.	All works areas for duration of works; Upon handover and completion of works.	
Table 15.6	12.3	-	CM2 - Reduction of construction period to practical minimum.	All works areas for duration of works; Upon handover and completion of works.	
Table 15.6	12.3	-	CM3 - Phasing of the construction stage to reduce visual impacts during the construction phase.	All works areas for duration of works; Upon handover and completion of works.	
Table 15.6	12.3	-	CM4 - Construction traffic (land and sea) including construction plants, construction vessels and barges should be kept to a practical minimum.	All works areas for duration of works; Upon handover and completion of works.	

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
Table 15.6	12.3	-	CM5 - Erection of decorative mesh screens or construction hoardings around works areas in visually unobtrusive colours.	All works areas for duration of works; Upon handover and completion of works. – may be disassembled in phases	I
Table 15.6	12.3	-	CM6 - Avoidance of excessive height and bulk of site buildings and structures.	New passenger concourse, terminal 2 expansion and other proposed airport related buildings and structures under the project; Upon handover and completion of works.	N/A
Table 15.6	12.3	-	CM7 - Control of night-time lighting by hooding all lights and through minimisation of night working periods.	All works areas for duration of works; Upon handover and completion of works. – may be disassembled in phases	I
Table 15.6	12.3	-	CM8 - All existing trees shall be carefully protected during construction. Detailed Tree Protection Specification shall be provided in the Contract Specification. Under this specification, the Contractor shall be required to submit, for approval, a detailed working method statement for the protection of trees prior to undertaking any works adjacent to all retained trees, including trees in contractor's works areas.	All existing trees to be retained; Upon handover and completion of works.	I
Table 15.6	12.3	-	CM9 - Trees unavoidably affected by the works shall be transplanted where practical. A detailed Tree Transplanting Specification shall be provided in the Contract Specification, if applicable. Sufficient time for necessary tree root and crown preparation periods shall be allowed in the project programme.	All existing trees to be affected by the works; Upon handover and completion of works.	I
Table 15.6	12.3	-	CM10 - Land formation works shall be followed with advanced hydroseeding around taxiways and runways as soon as practical.	All affected existing grass areas around runways and verges/Duration of works;	N/A

EIA Ref.	EM&A Ref.	EP Condition	Environmental Protection Measures	Location / Duration of measures Timing of completion of measures	Mitigation Measures Implemented?^
				Upon handover and completion of works.	
			Cultural Heritage Impact – Construction Phase		
			Not applicable.		
			Health Impact – Aircraft Emissions		
			Not applicable.		
			Health Impact – Aircraft Noise		
			Not applicable.		

Notes:

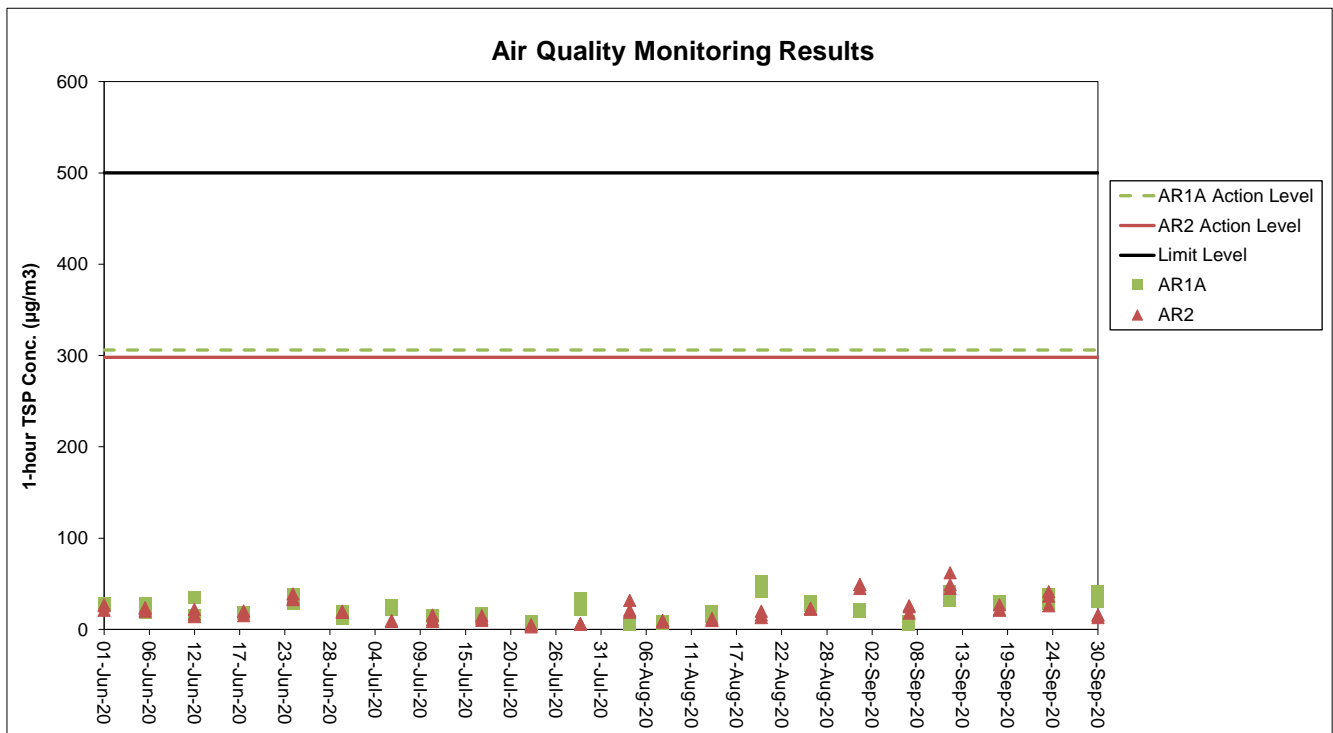
I= implemented where applicable;

N/A= not applicable to the construction works implemented during the reporting month.

^ Checked by ET through site inspection and record provided by the Contractor.

Appendix C. Monitoring Results

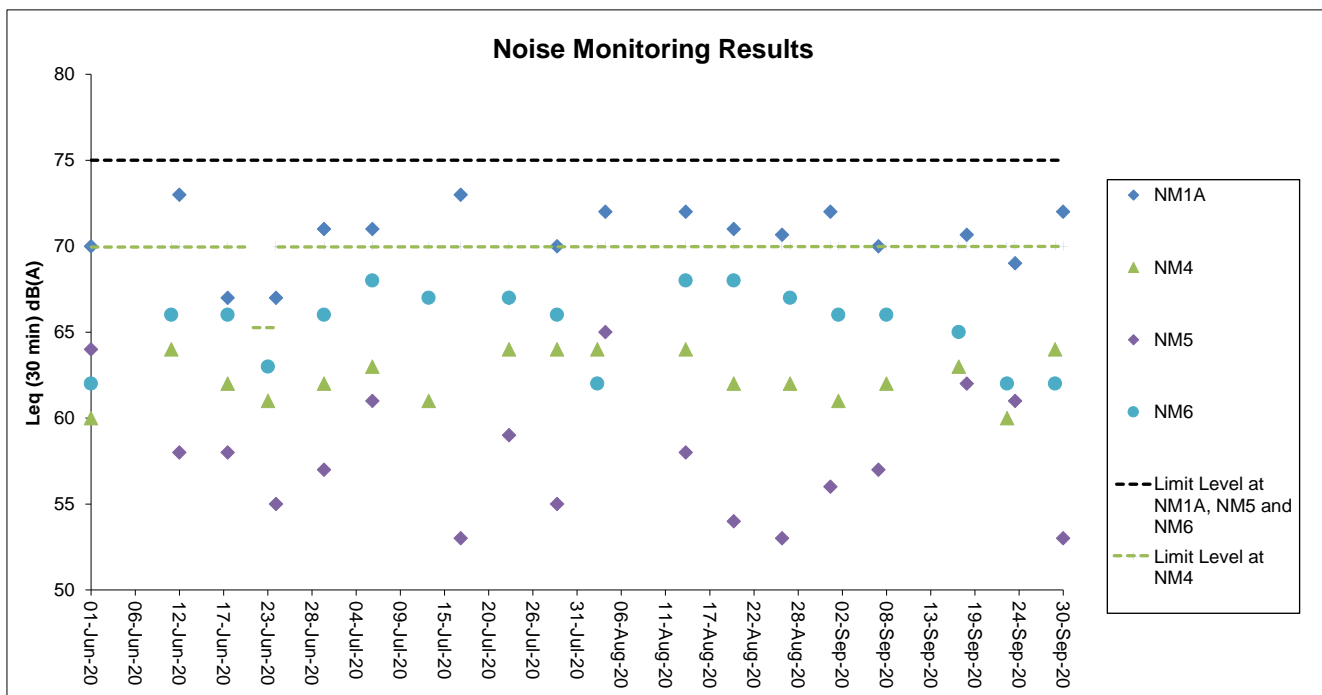
Air Quality Monitoring Results



Notes:

1. The key activities of the Project carried out in the reporting period included reclamation works and land-based works. Works in the reclamation areas included DCM works, marine filling, seawall and facilities construction, together with runway and associated works. Land-based works on existing airport island involved mainly airfield works, foundation and substructure work for Terminal 2 expansion, modification and tunnel work for APM and BHS systems, and preparation work for utilities, with activities include site establishment, site office construction, road and drainage works, cable ducting, demolition of existing facilities, piling, and excavation works.
2. General weather condition during monitoring ranged from sunny to cloudy. Detailed meteorological conditions can be referred to Table 2.3 of this Report and corresponding Monthly EM&A Reports.
3. QA/ QC requirements as stipulated in the EM&A Manual were carried out during measurement.

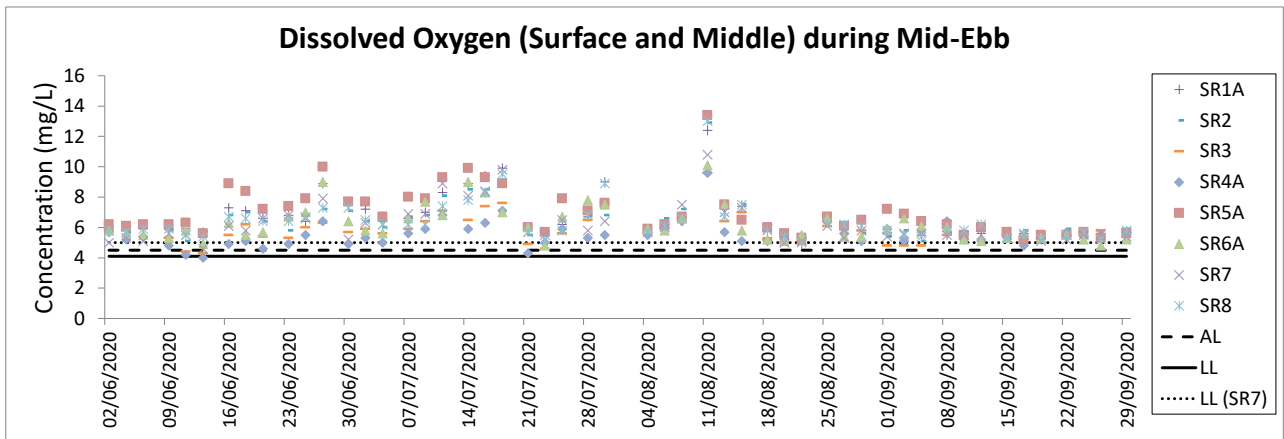
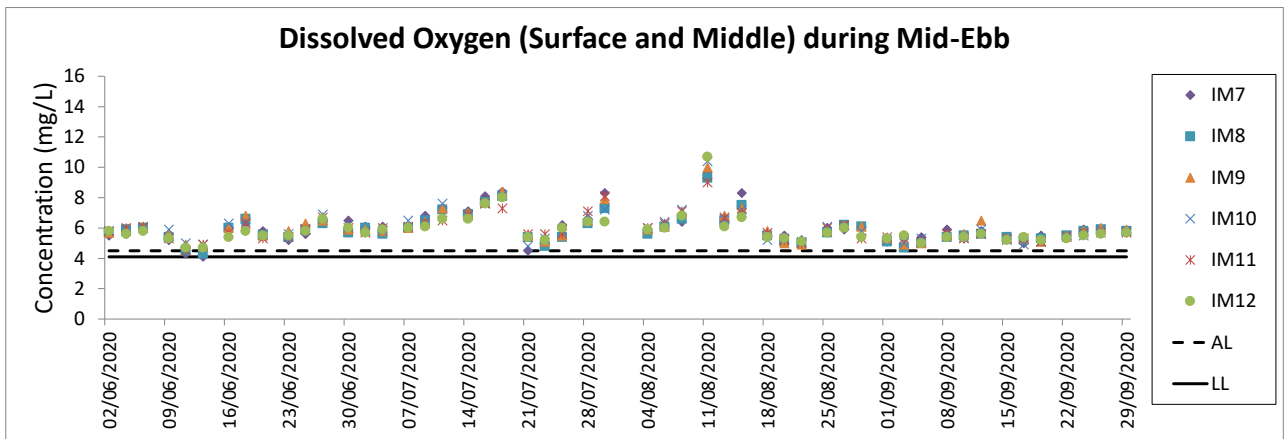
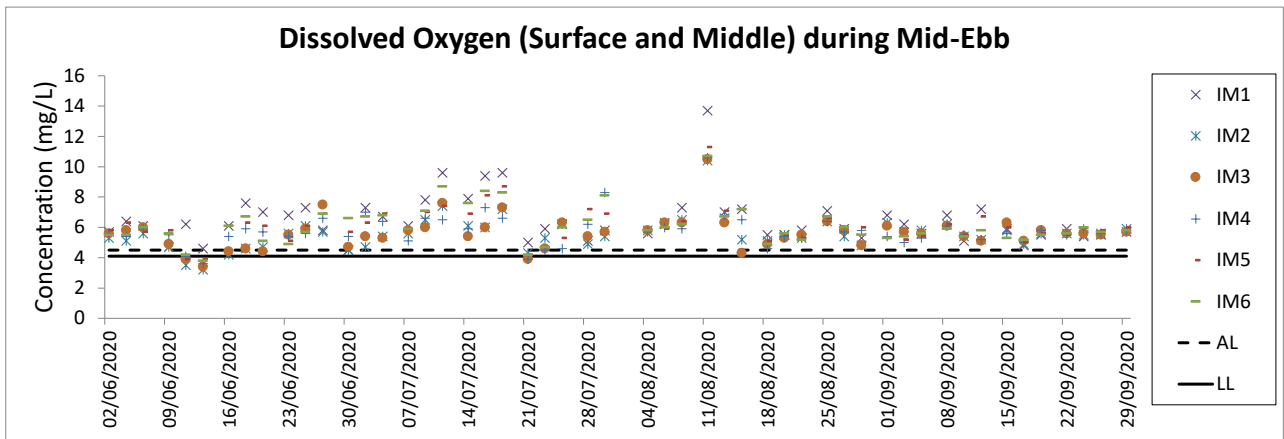
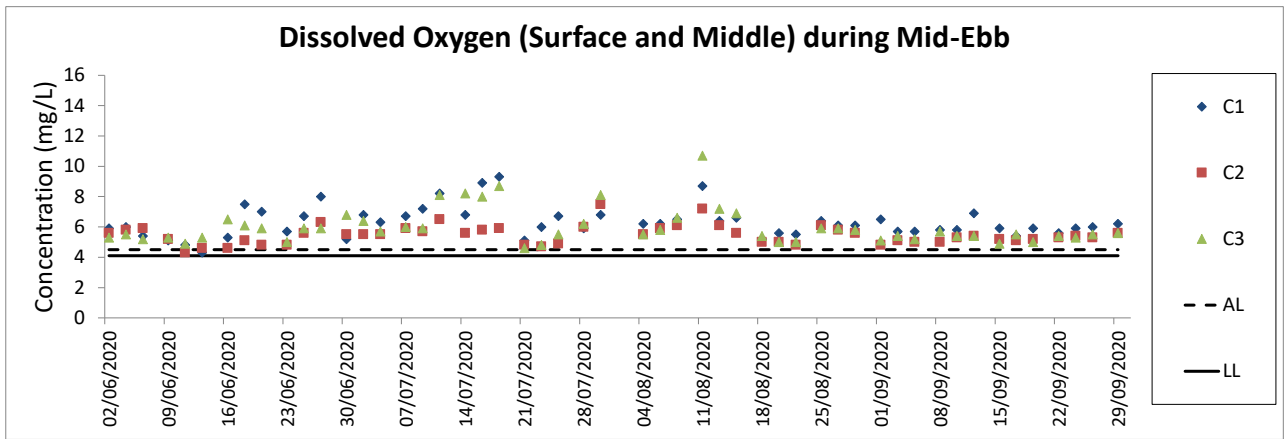
Noise Monitoring Results



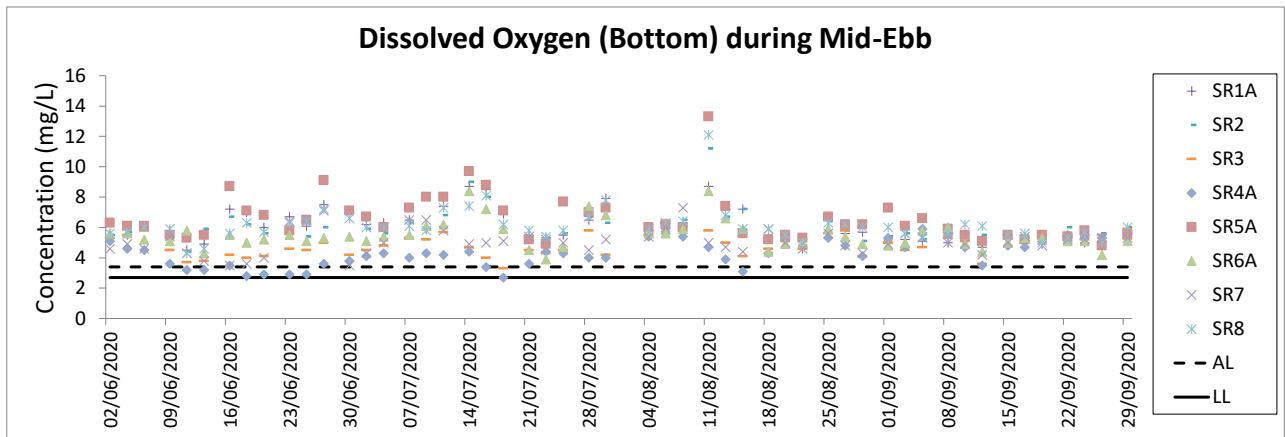
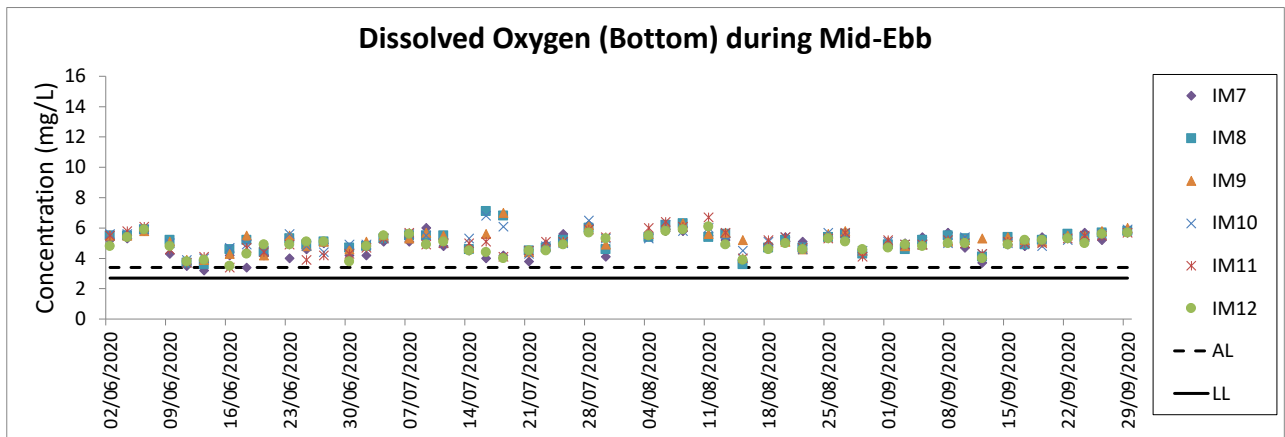
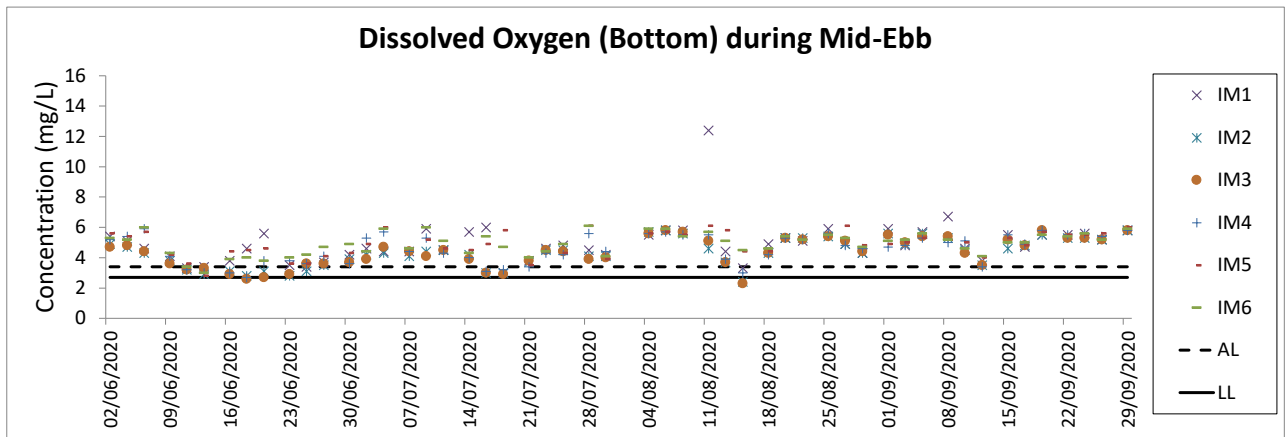
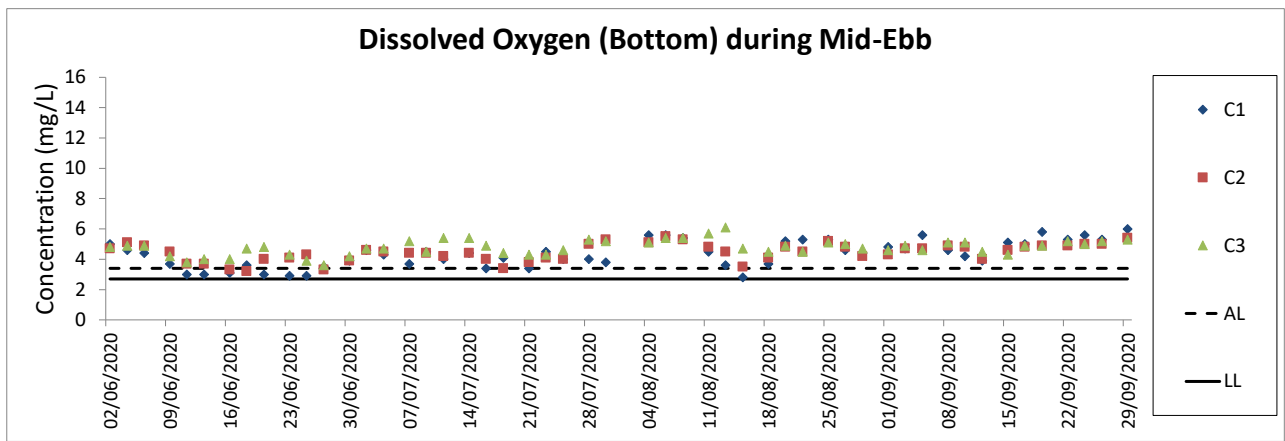
Notes:

1. The Limit Level is reduced to 70dB(A) for school and 65dB(A) during school examination period at NM4. School examination took place from 22 to 23 June during this reporting period.
2. The key activities of the Project carried out in the reporting period included reclamation works and land-based works. Works in the reclamation areas included DCM works, marine filling, seawall and facilities construction, together with runway and associated works. Land-based works on existing airport island involved mainly airfield works, foundation and substructure work for Terminal 2 expansion, modification and tunnel work for APM and BHS systems, and preparation work for utilities, with activities include site establishment, site office construction, road and drainage works, cable ducting, demolition of existing facilities, piling, and excavation works.
3. General weather condition during monitoring ranged from sunny to cloudy. Detailed meteorological conditions can be referred to Table 2.6 of this Report and corresponding Monthly EM&A Reports.
4. QA/ QC requirements as stipulated in the EM&A Manual were carried out during measurement.

Water Quality Monitoring Results

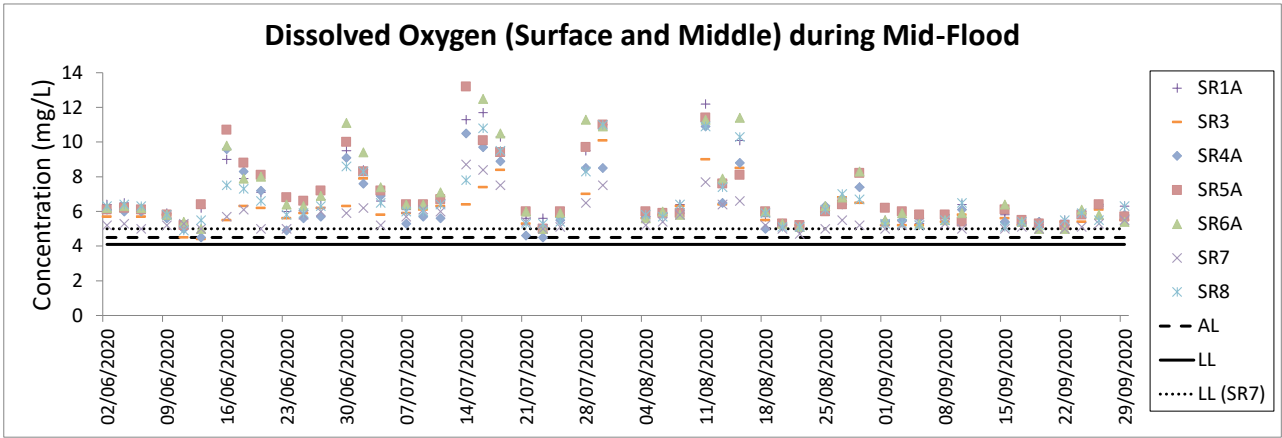
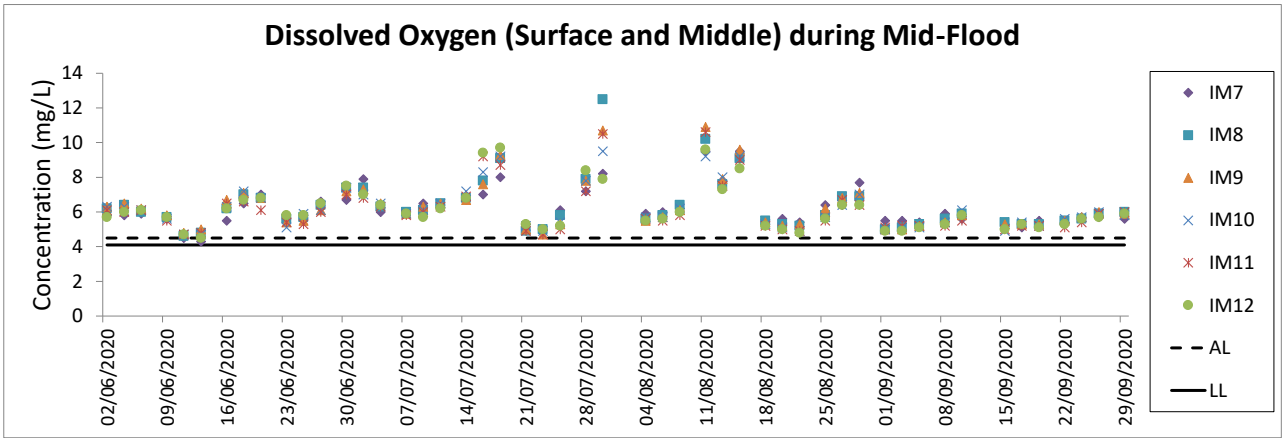
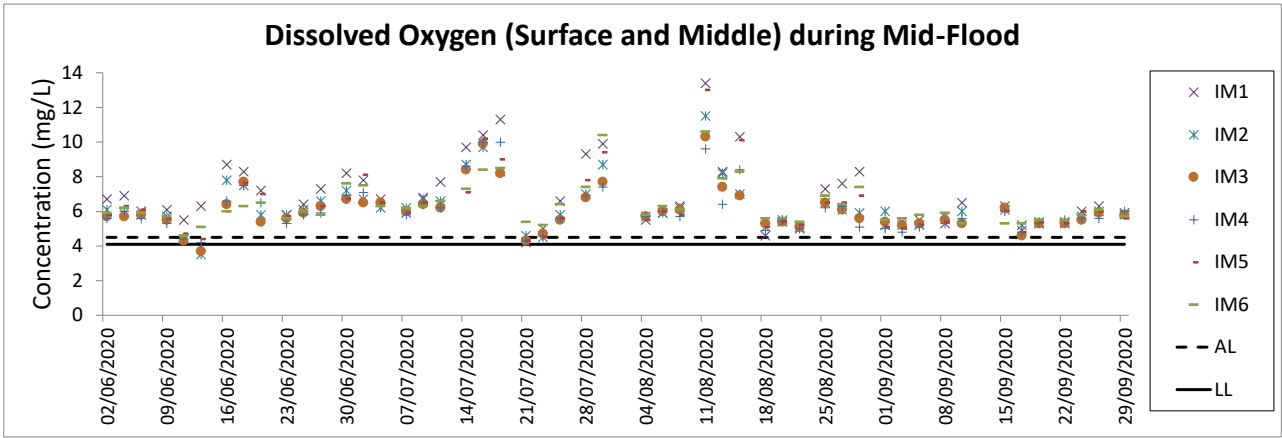
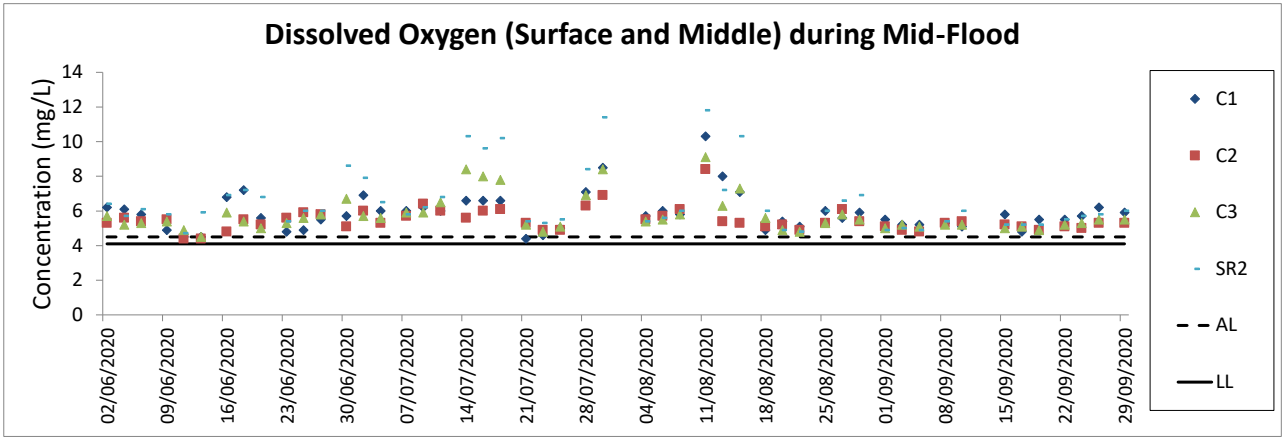


- Notes:
1. The key marine works activities of the Project during monitoring included deep cement mixing (DCM) works, marine filling, seawall and facilities construction, together with runway and associated works.
 2. General weather condition during monitoring ranged from sunny to rainy, with sea condition ranged from calm to rough. Detailed meteorological conditions can be referred to Table 2.11 of this Report and corresponding Monthly EM&A Reports.
 3. QA/ QC requirements as stipulated in the EM&A Manual were carried out during measurement.

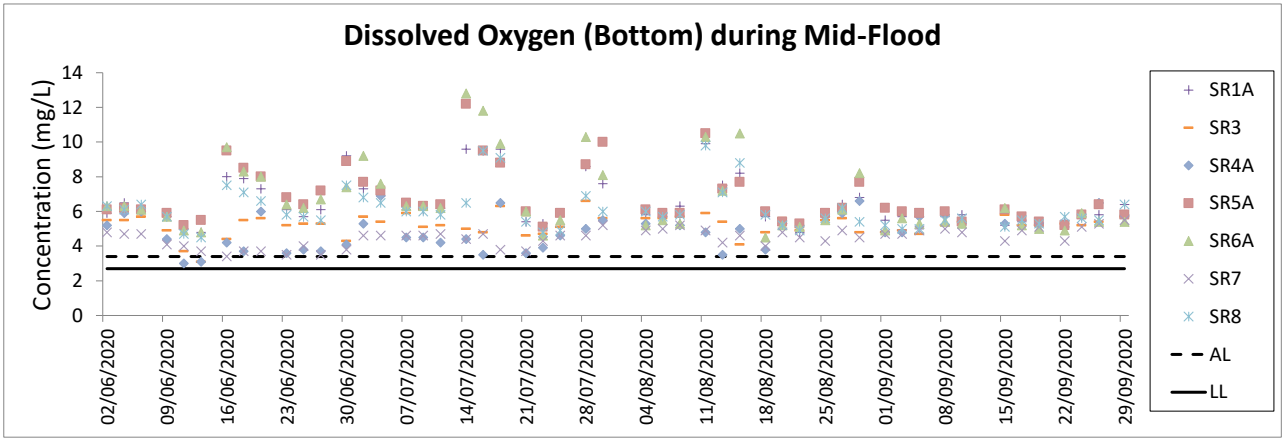
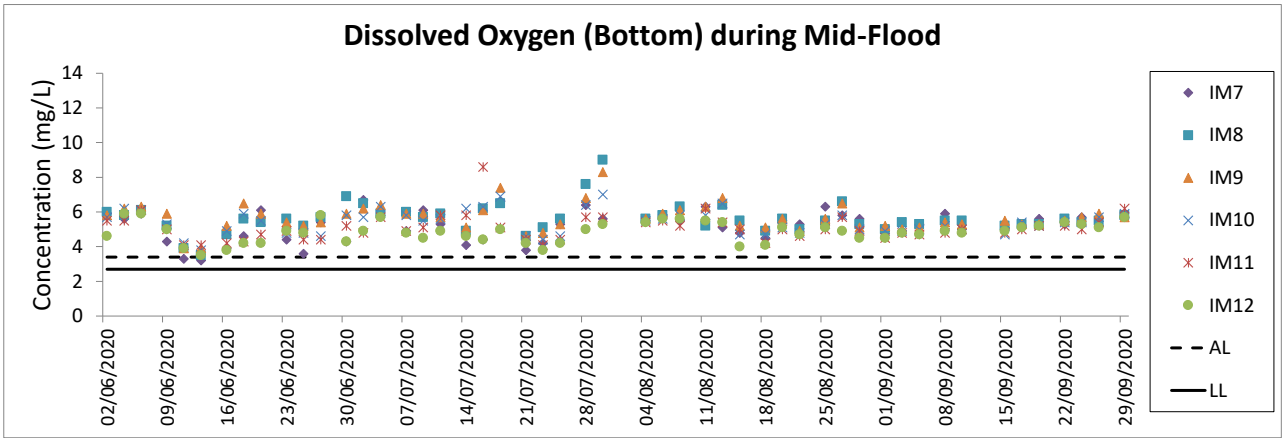
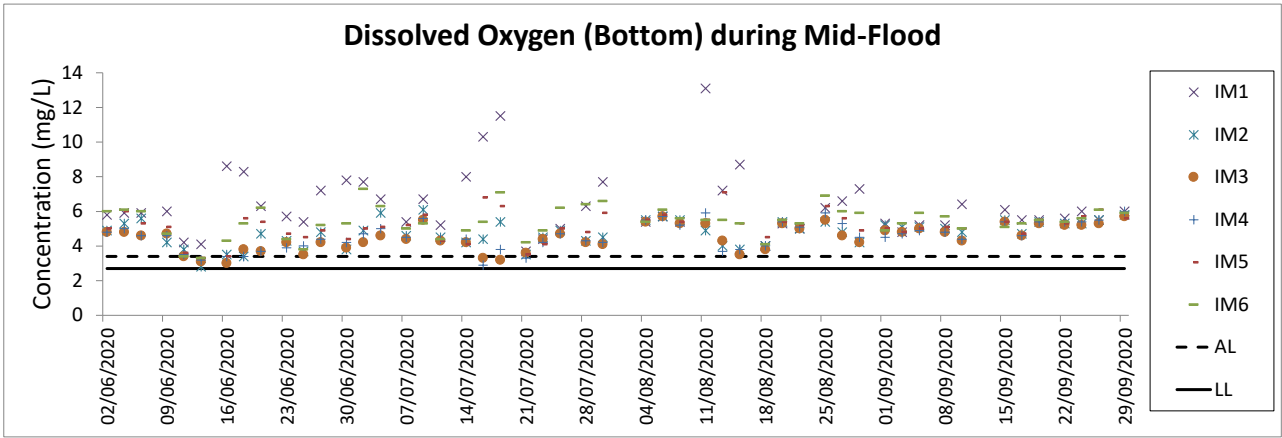
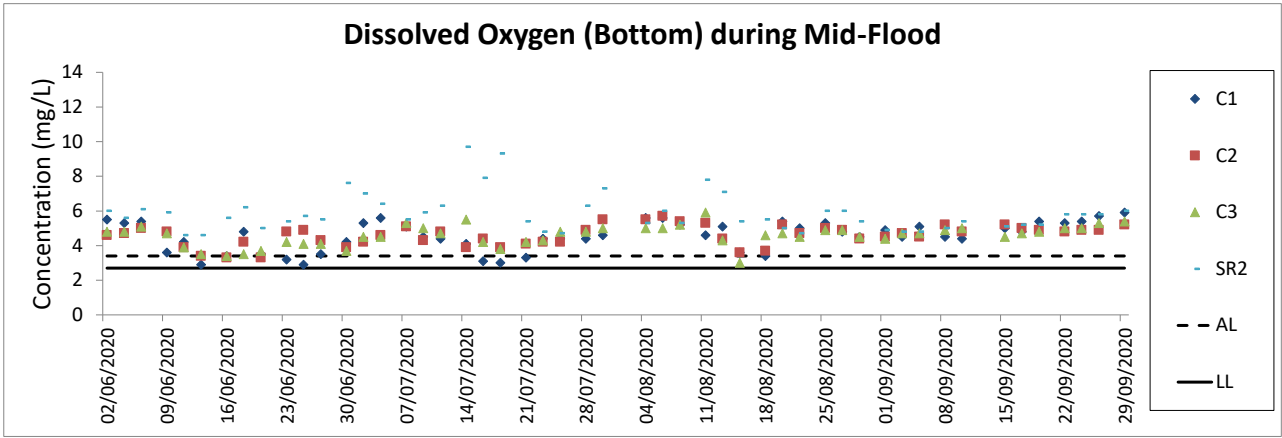


Notes:

1. The key marine works activities of the Project during monitoring included deep cement mixing (DCM) works, marine filling, seawall and facilities construction, together with runway and associated works.
2. General weather condition during monitoring ranged from sunny to rainy, with sea condition ranged from calm to rough. Detailed meteorological conditions can be referred to Table 2.11 of this Report and corresponding Monthly EM&A Reports.
3. QA/ QC requirements as stipulated in the EM&A Manual were carried out during measurement.

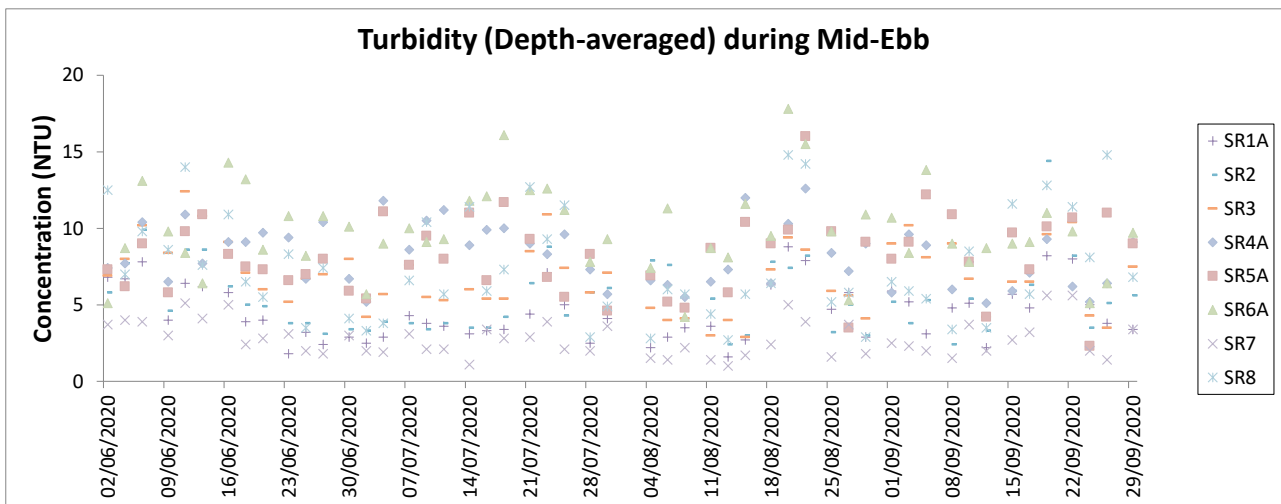
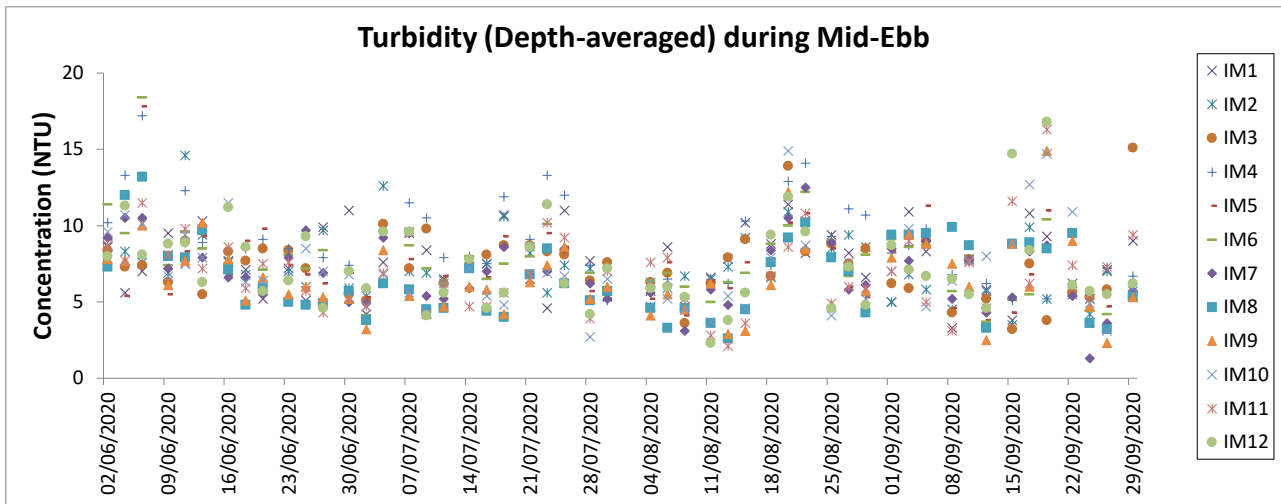
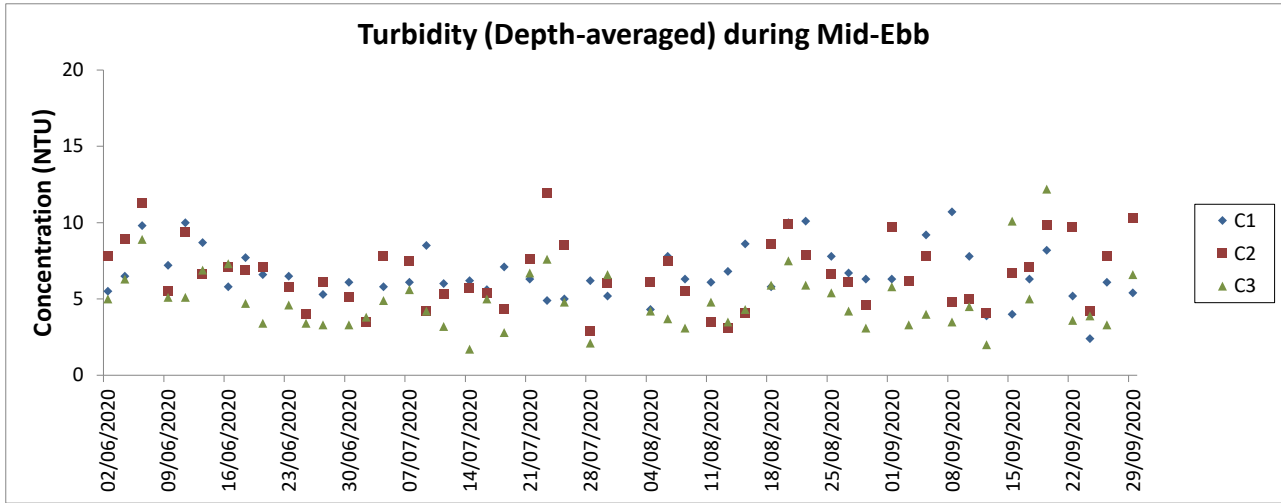


- Notes:**
- The key marine works activities of the Project during monitoring included deep cement mixing (DCM) works, marine filling, seawall and facilities construction, together with runway and associated works.
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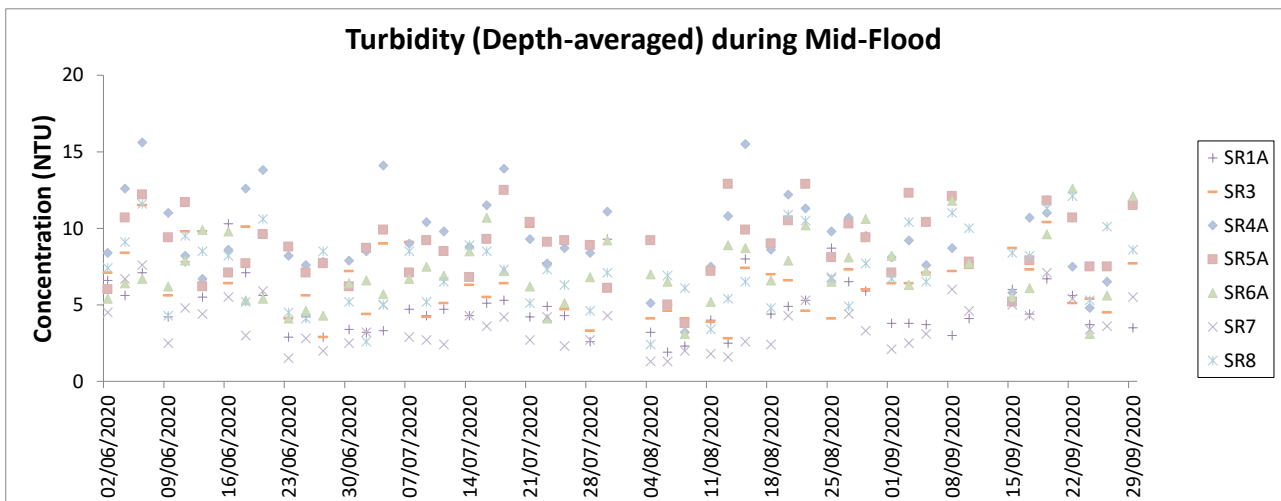
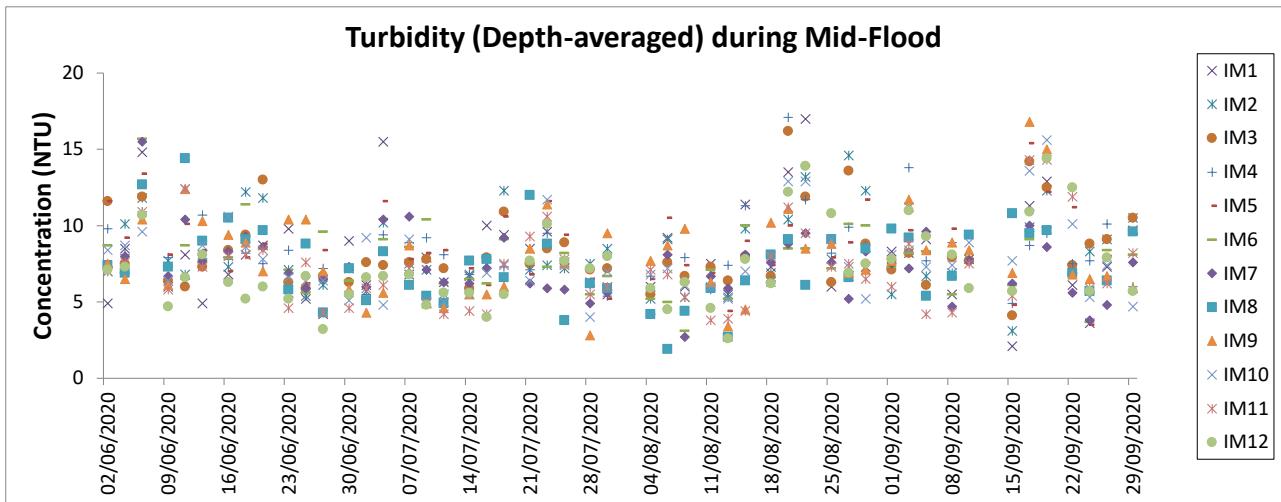
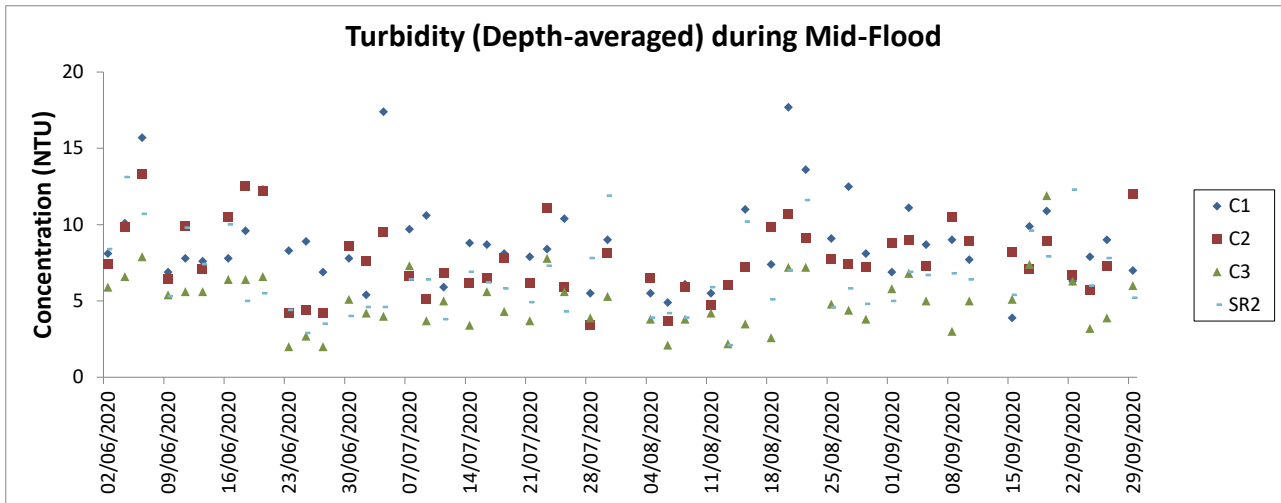


Notes:

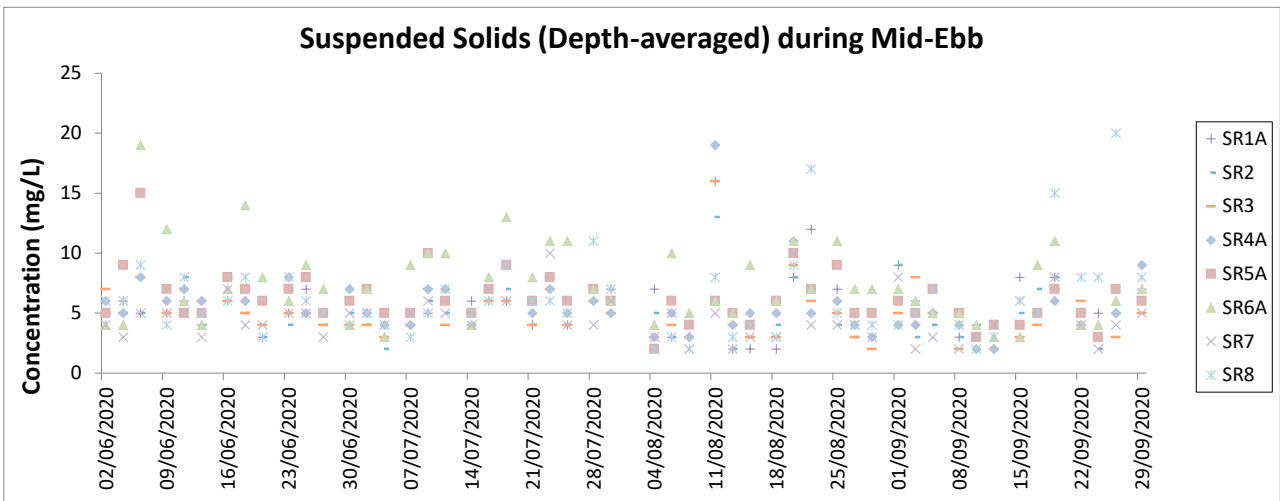
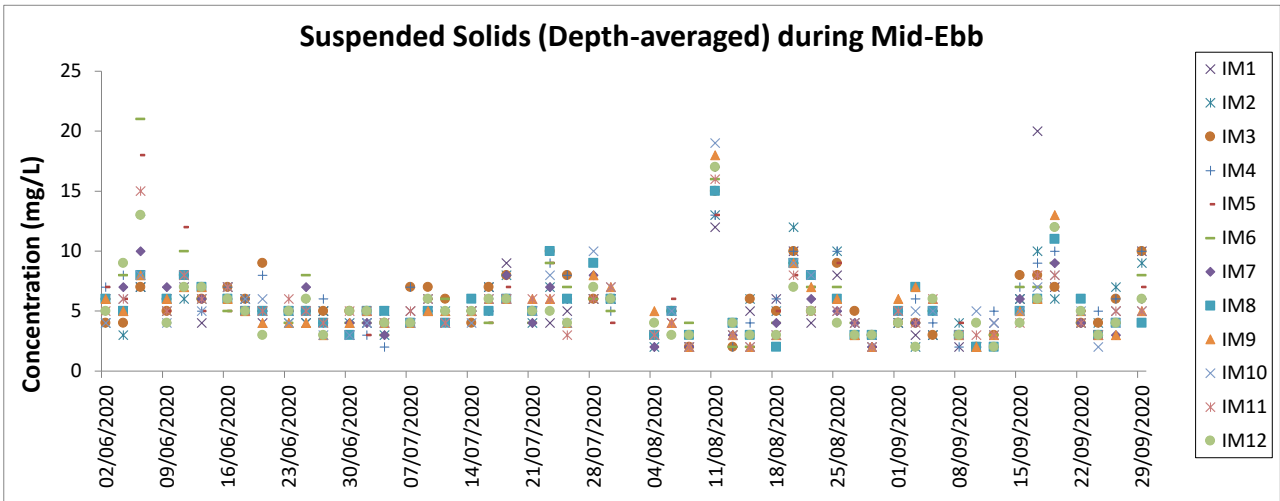
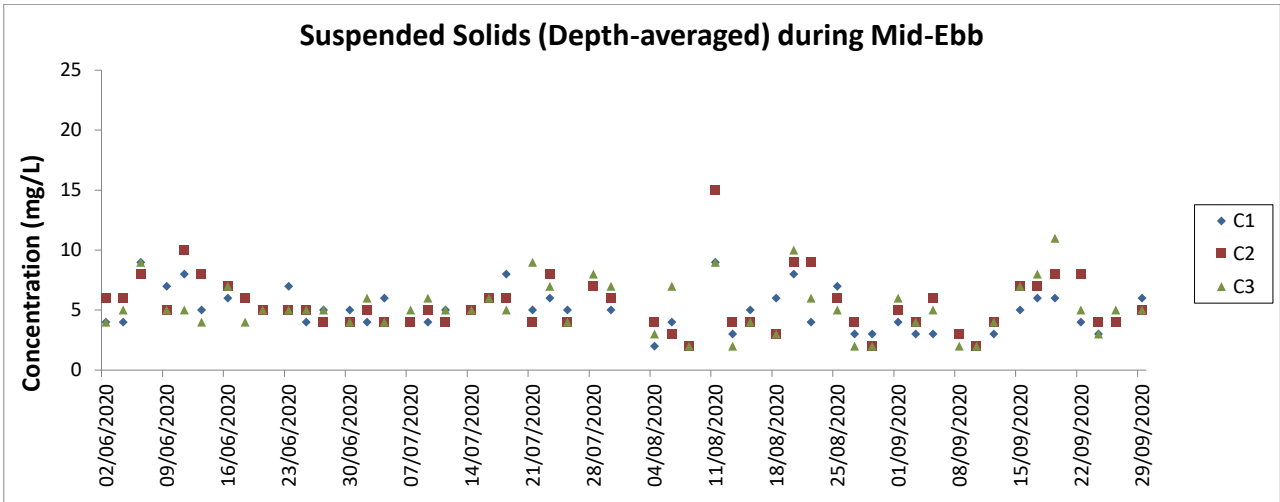
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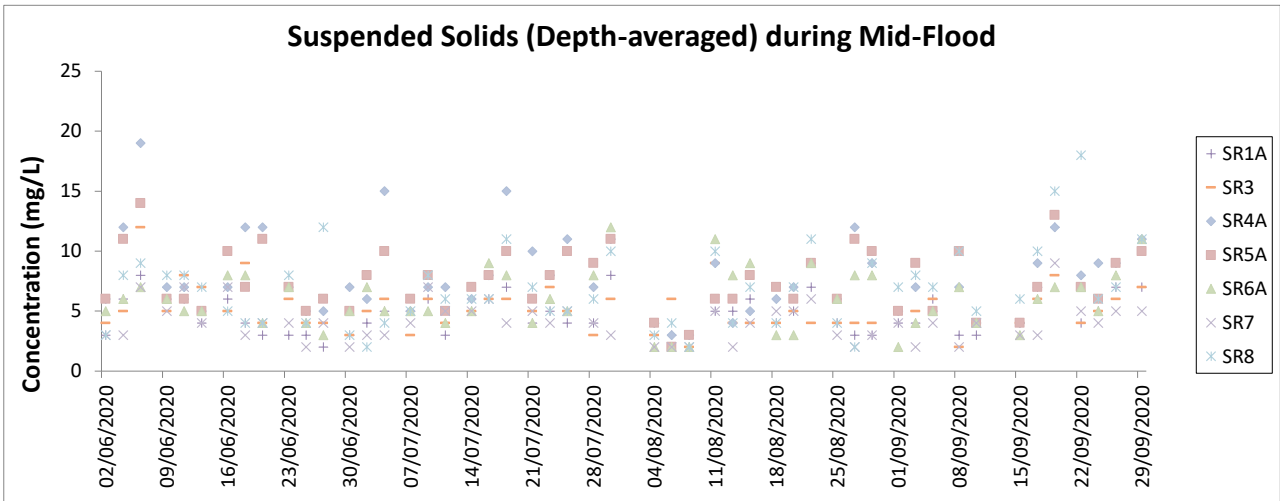
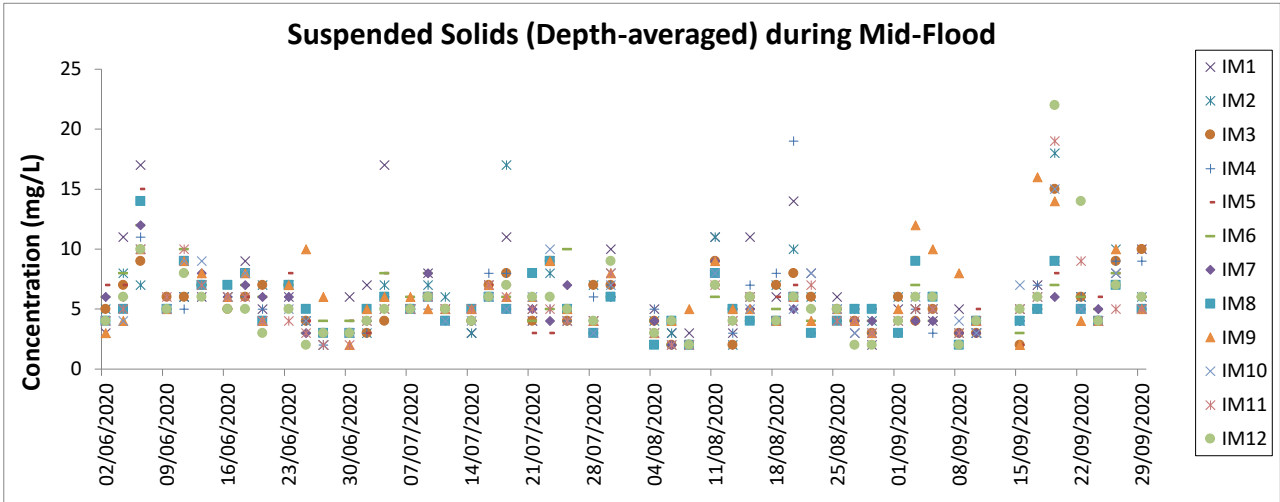
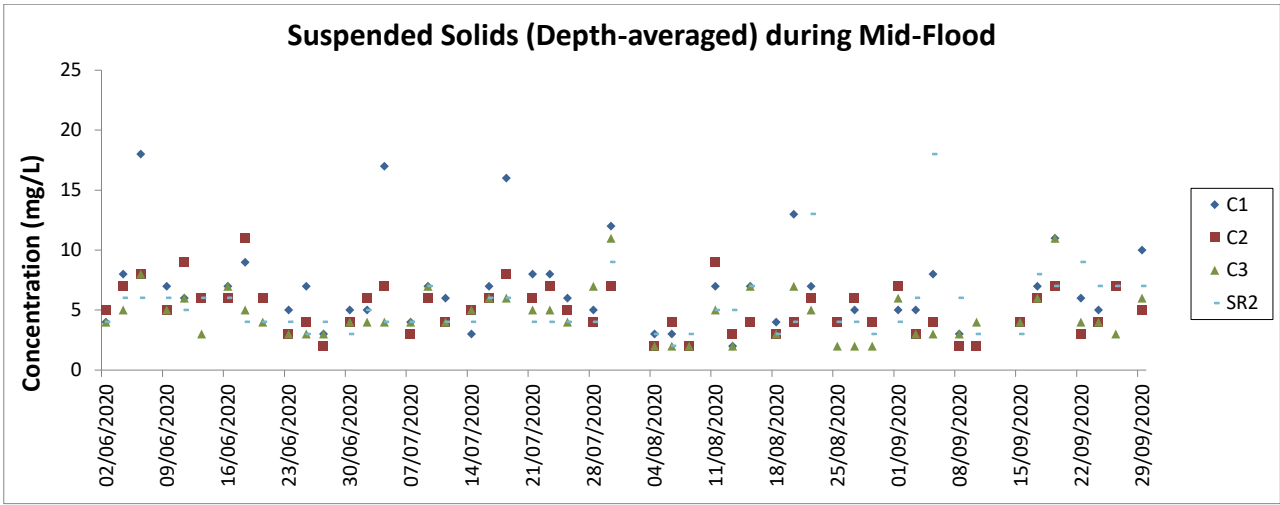
- Notes:
1. The Action and Limit Levels can be referred to Table 2.8 of this Report.
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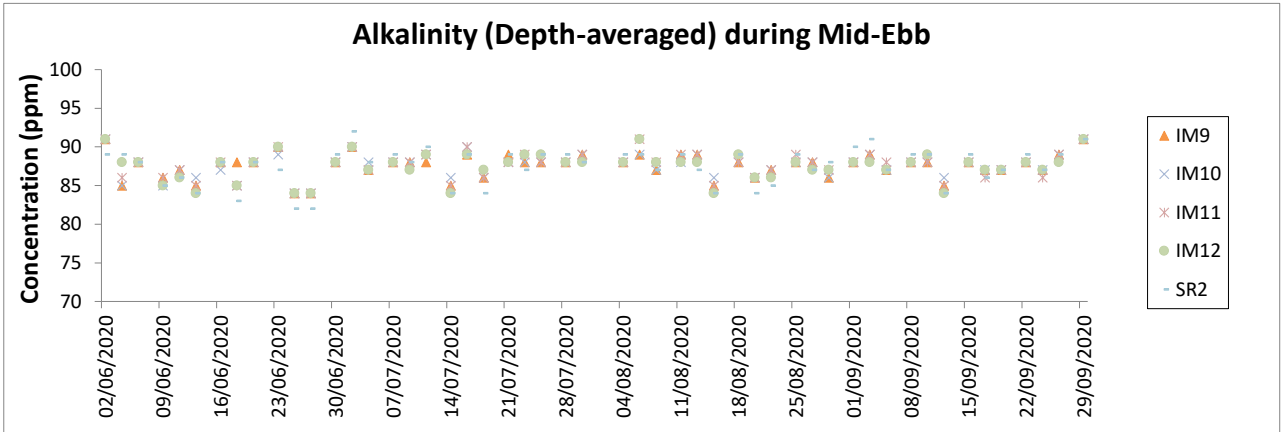
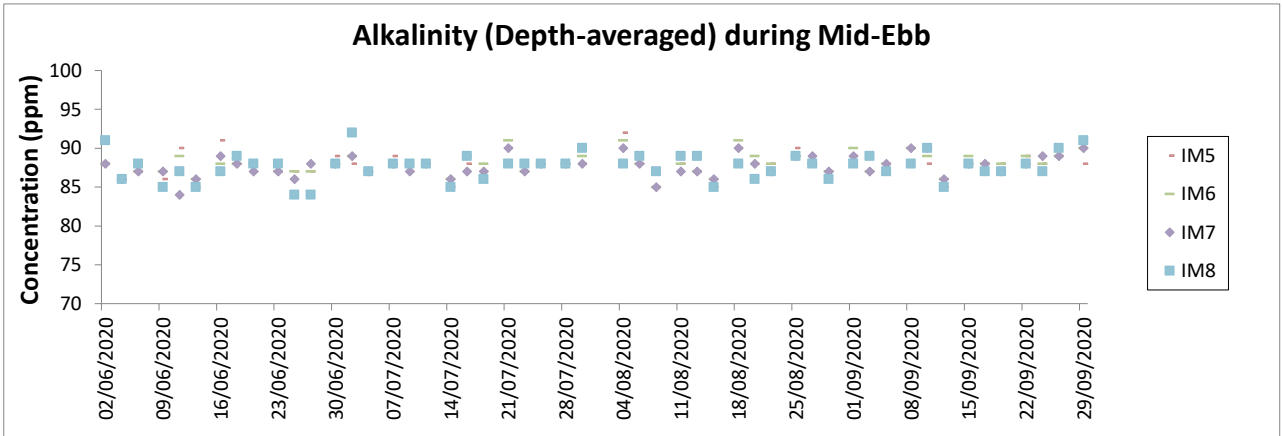
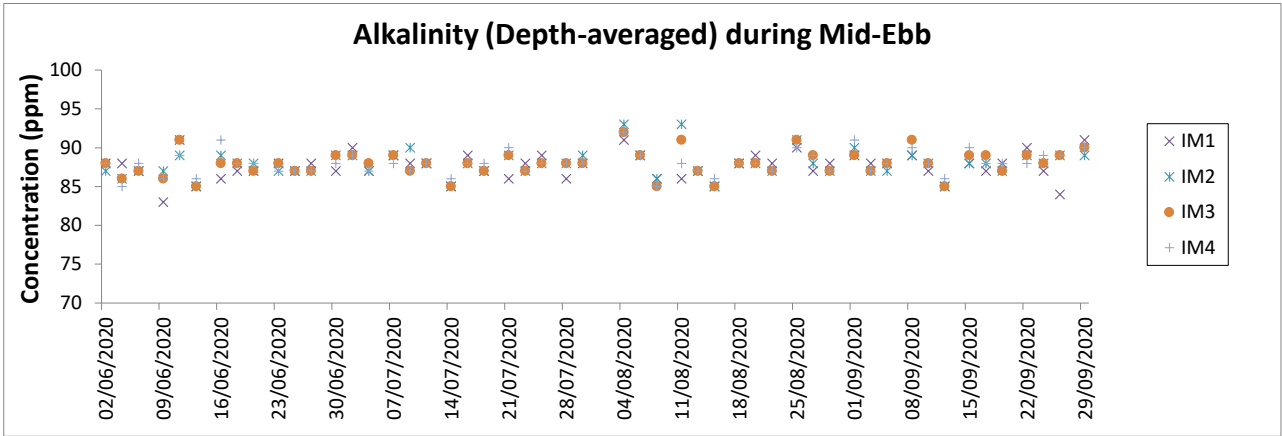
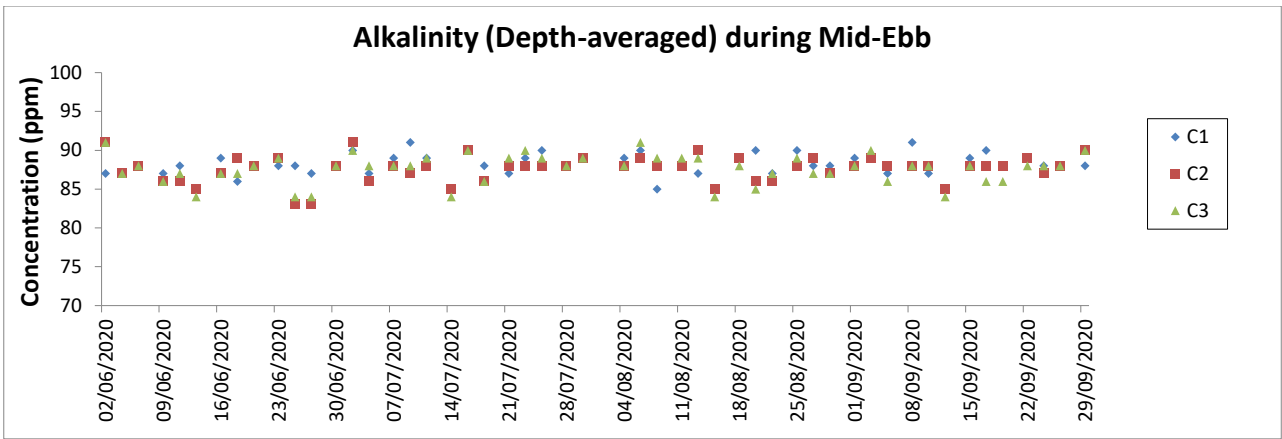
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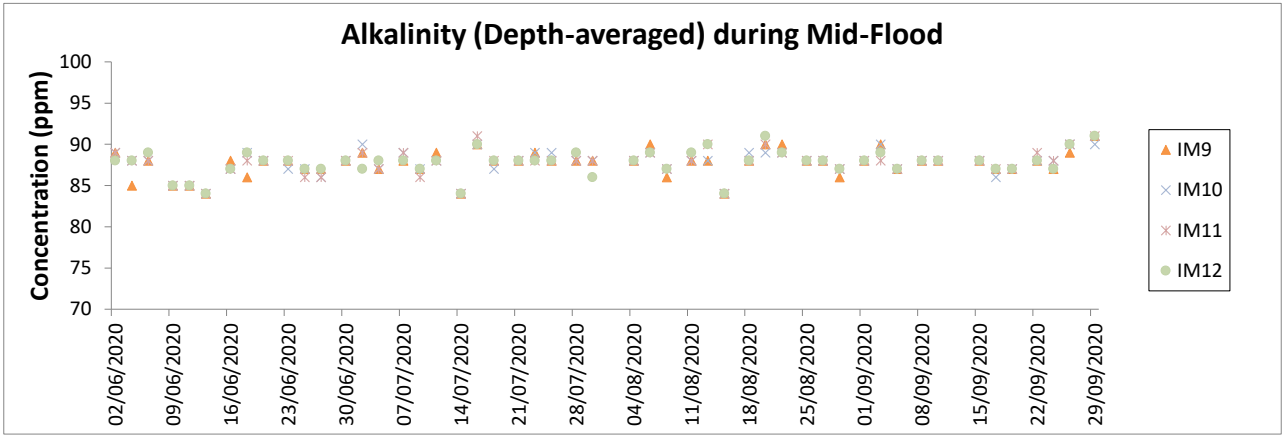
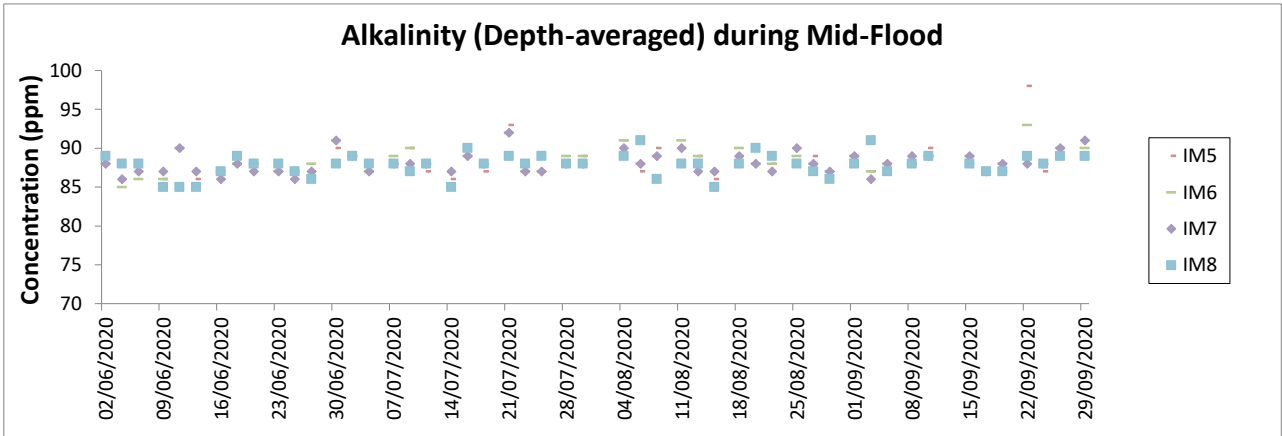
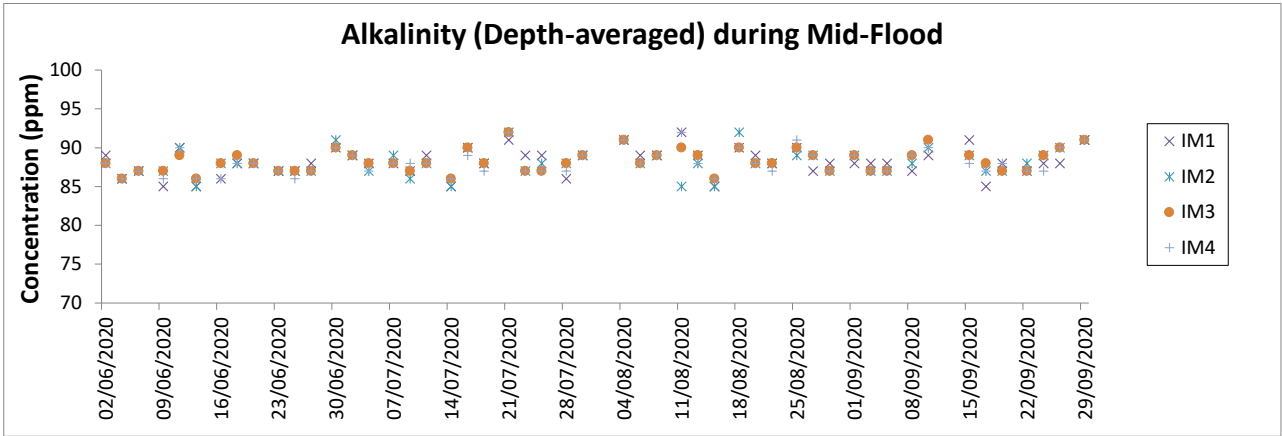
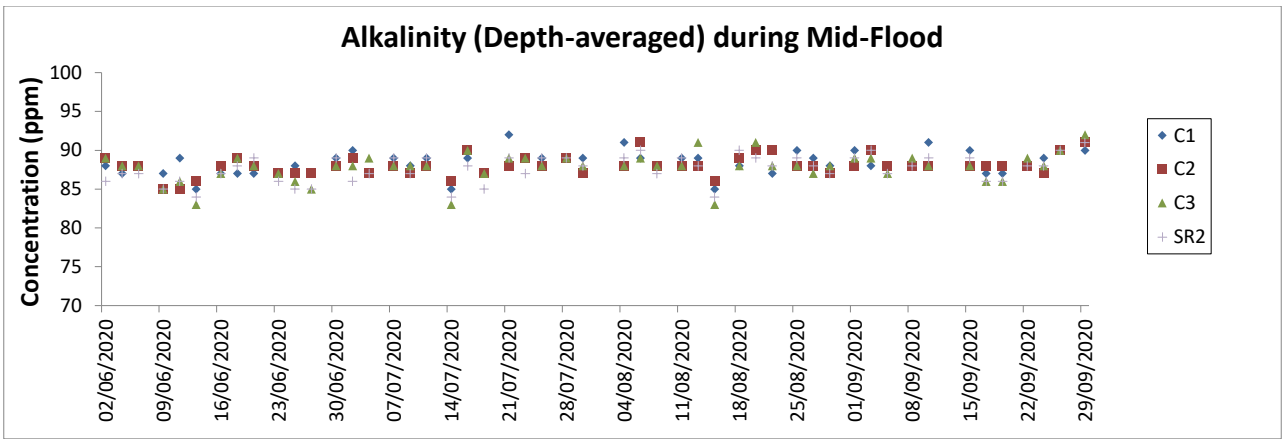


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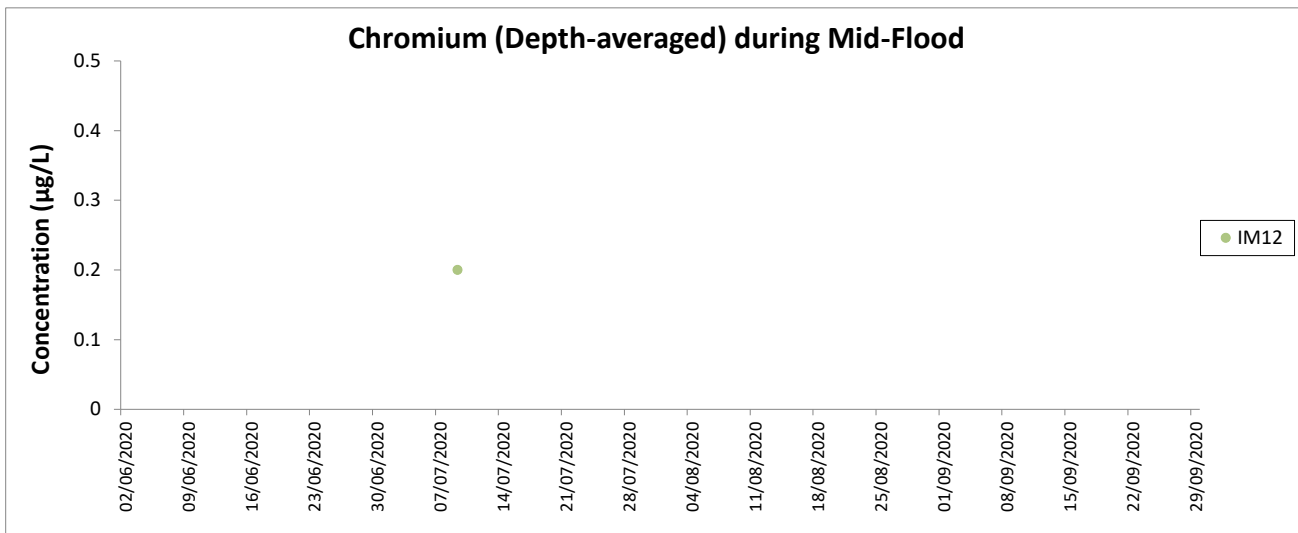
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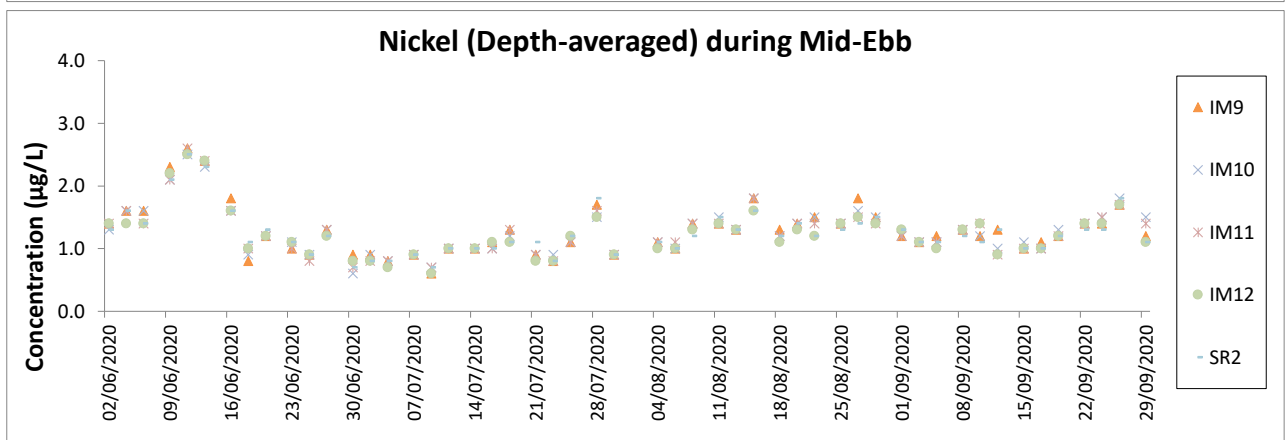
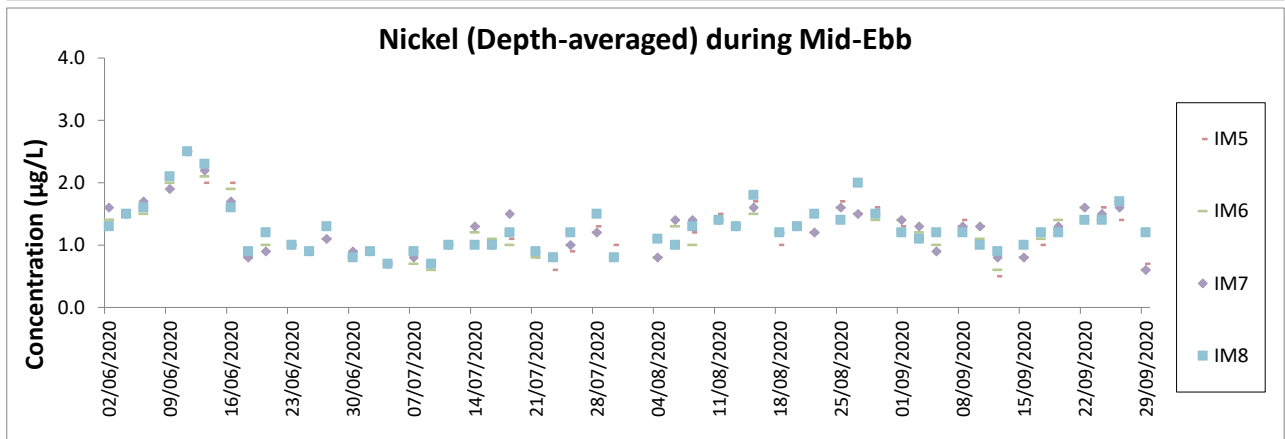
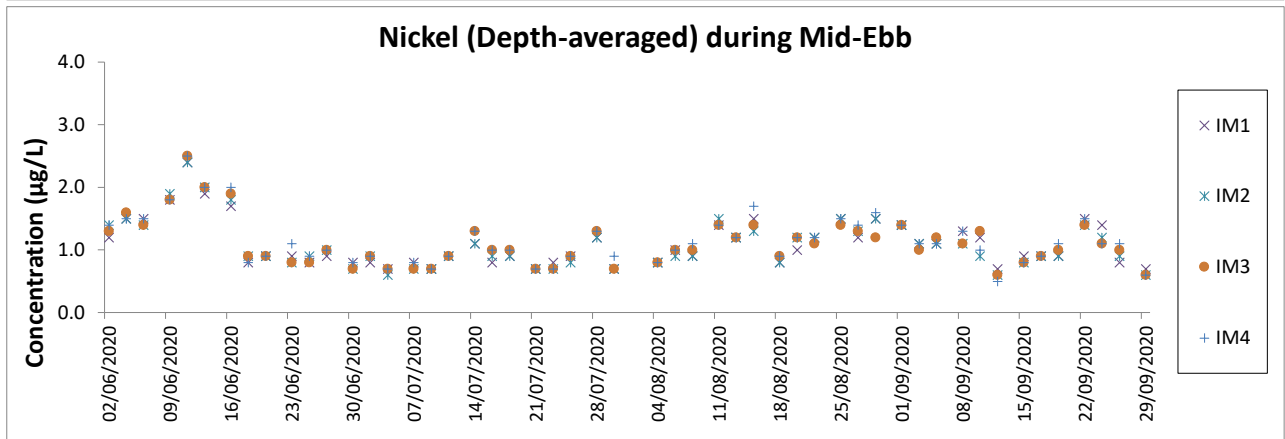
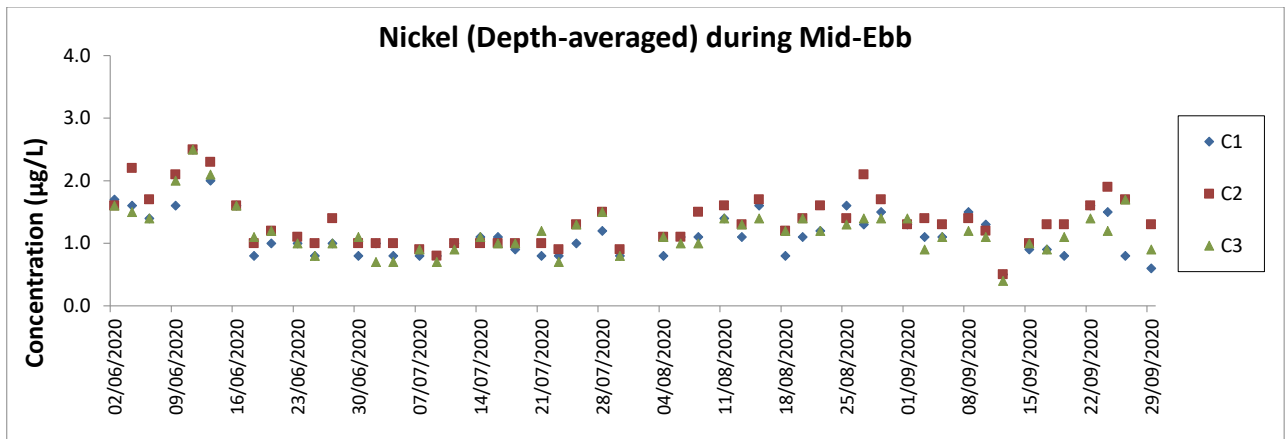
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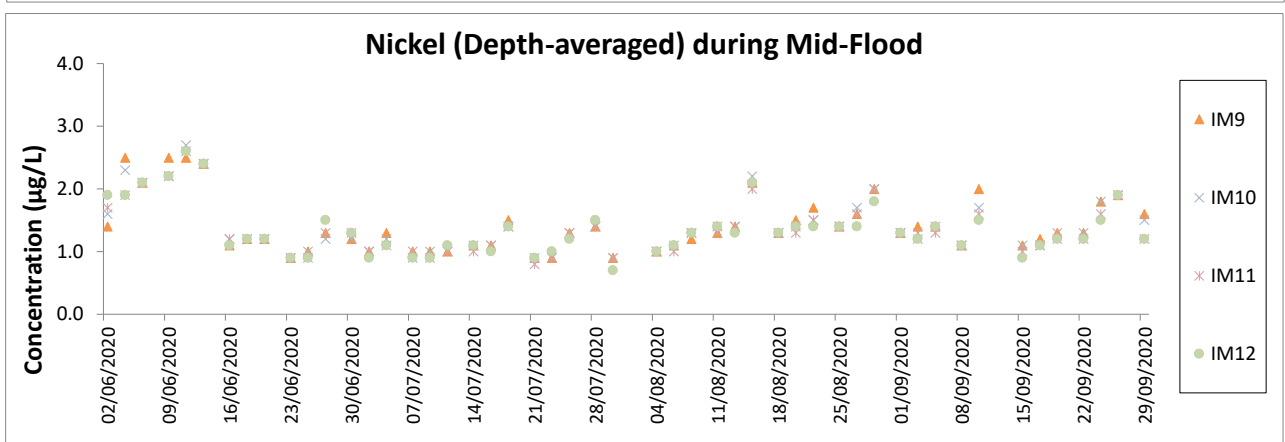
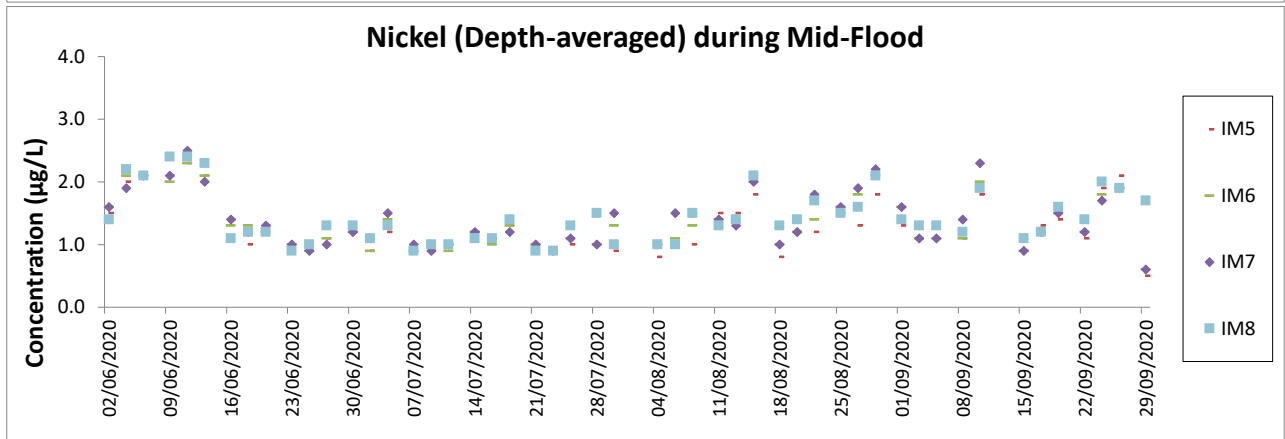
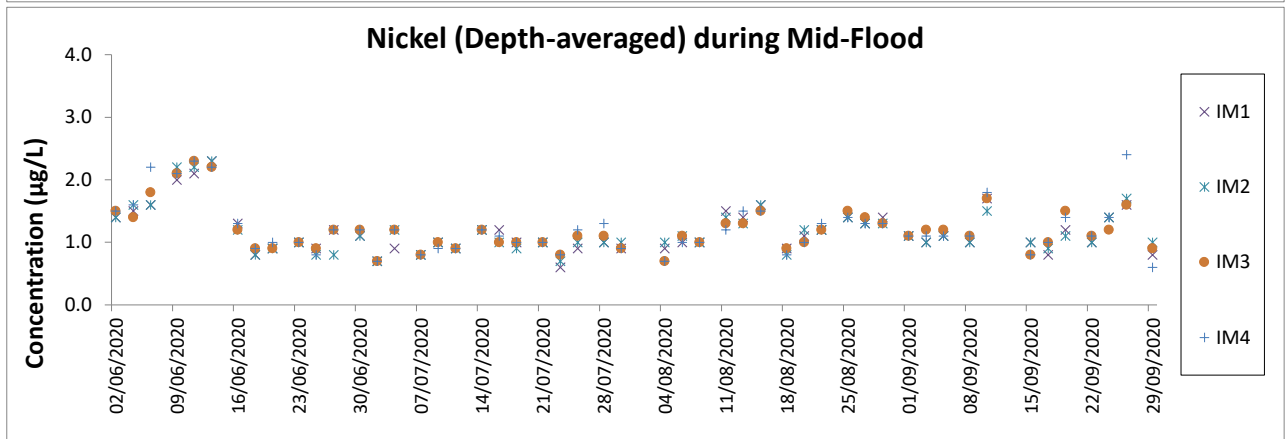
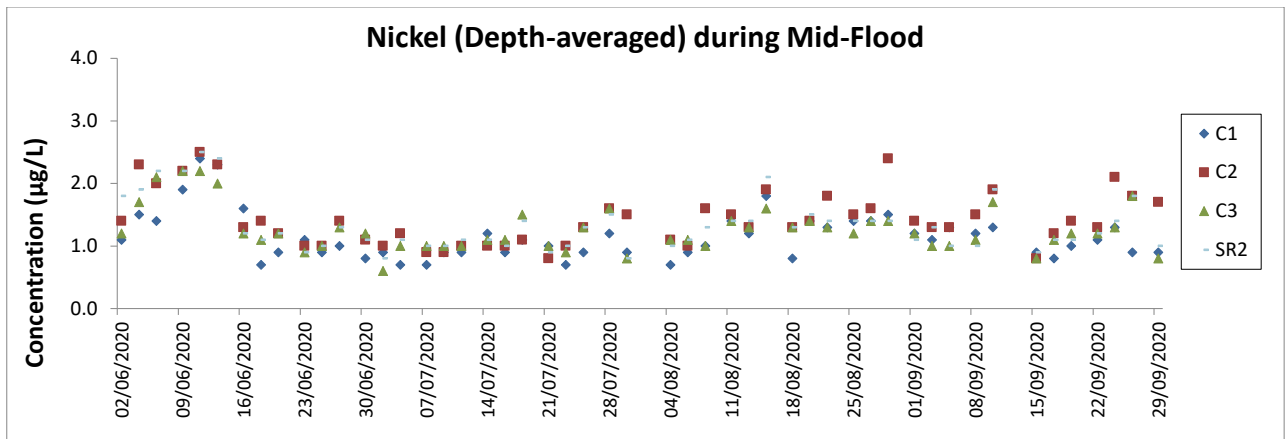
Notes:

1. The Action and Limit Levels can be referred to Table 2.8 of this Report.
2. The monitoring results of chromium not presented above were below the reporting limit of 0.2 µg/L.
3. The key marine works activities of the Project during monitoring included deep cement mixing (DCM) works, marine filling, seawall and facilities construction, together with runway
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Chinese White Dolphin Monitoring Results

CWD Small Vessel Line-transect Survey

Survey Effort Data

DATE	AREA	BEAU	KM SEARCHED	SEASON	VESSEL	TYPE	P/S
6-Jul-20	AW	3	4.900	SUMMER	32166	3RS ET	P
6-Jul-20	WL	3	14.052	SUMMER	32166	3RS ET	P
6-Jul-20	WL	4	4.029	SUMMER	32166	3RS ET	P
6-Jul-20	WL	3	5.088	SUMMER	32166	3RS ET	S
6-Jul-20	WL	4	3.731	SUMMER	32166	3RS ET	S
8-Jul-20	NEL	2	0.500	SUMMER	32166	3RS ET	P
8-Jul-20	NEL	3	33.650	SUMMER	32166	3RS ET	P
8-Jul-20	NEL	4	3.230	SUMMER	32166	3RS ET	P
8-Jul-20	NEL	2	2.000	SUMMER	32166	3RS ET	S
8-Jul-20	NEL	3	7.720	SUMMER	32166	3RS ET	S
9-Jul-20	NEL	2	1.300	SUMMER	32166	3RS ET	P
9-Jul-20	NEL	3	25.670	SUMMER	32166	3RS ET	P
9-Jul-20	NEL	4	9.820	SUMMER	32166	3RS ET	P
9-Jul-20	NEL	2	1.000	SUMMER	32166	3RS ET	S
9-Jul-20	NEL	3	9.910	SUMMER	32166	3RS ET	S
10-Jul-20	NWL	3	49.090	SUMMER	32166	3RS ET	P
10-Jul-20	NWL	4	14.710	SUMMER	32166	3RS ET	P
10-Jul-20	NWL	2	2.100	SUMMER	32166	3RS ET	S
10-Jul-20	NWL	3	10.000	SUMMER	32166	3RS ET	S
13-Jul-20	AW	2	0.980	SUMMER	32166	3RS ET	P
13-Jul-20	AW	3	3.950	SUMMER	32166	3RS ET	S
13-Jul-20	WL	2	7.997	SUMMER	32166	3RS ET	P
13-Jul-20	WL	3	6.388	SUMMER	32166	3RS ET	P
13-Jul-20	WL	2	2.175	SUMMER	32166	3RS ET	S
13-Jul-20	WL	3	5.392	SUMMER	32166	3RS ET	S
20-Jul-20	SWL	2	44.018	SUMMER	32166	3RS ET	P
20-Jul-20	SWL	3	3.890	SUMMER	32166	3RS ET	P
20-Jul-20	SWL	2	12.803	SUMMER	32166	3RS ET	S
20-Jul-20	SWL	3	1.000	SUMMER	32166	3RS ET	S
21-Jul-20	SWL	1	8.130	SUMMER	32166	3RS ET	P
21-Jul-20	SWL	2	26.735	SUMMER	32166	3RS ET	P
21-Jul-20	SWL	3	15.310	SUMMER	32166	3RS ET	P
21-Jul-20	SWL	1	1.034	SUMMER	32166	3RS ET	S
21-Jul-20	SWL	2	12.790	SUMMER	32166	3RS ET	S
21-Jul-20	SWL	3	0.920	SUMMER	32166	3RS ET	S
22-Jul-20	NWL	1	14.280	SUMMER	32166	3RS ET	P
22-Jul-20	NWL	2	35.930	SUMMER	32166	3RS ET	P
22-Jul-20	NWL	3	12.500	SUMMER	32166	3RS ET	P
22-Jul-20	NWL	1	1.300	SUMMER	32166	3RS ET	S
22-Jul-20	NWL	2	9.190	SUMMER	32166	3RS ET	S
22-Jul-20	NWL	3	1.100	SUMMER	32166	3RS ET	S
7-Aug-20	AW	2	4.830	SUMMER	32166	3RS ET	P
7-Aug-20	WL	2	11.333	SUMMER	32166	3RS ET	P
7-Aug-20	WL	3	8.330	SUMMER	32166	3RS ET	P
7-Aug-20	WL	2	2.260	SUMMER	32166	3RS ET	S

DATE	AREA	BEAU	KM SEARCHED	SEASON	VESSEL	TYPE	P/S
7-Aug-20	WL	3	4.810	SUMMER	32166	3RS ET	S
10-Aug-20	SWL	2	36.803	SUMMER	32166	3RS ET	P
10-Aug-20	SWL	3	14.500	SUMMER	32166	3RS ET	P
10-Aug-20	SWL	2	13.697	SUMMER	32166	3RS ET	S
10-Aug-20	SWL	3	3.100	SUMMER	32166	3RS ET	S
11-Aug-20	NWL	2	18.930	SUMMER	32166	3RS ET	P
11-Aug-20	NWL	3	41.090	SUMMER	32166	3RS ET	P
11-Aug-20	NWL	4	3.780	SUMMER	32166	3RS ET	P
11-Aug-20	NWL	2	5.600	SUMMER	32166	3RS ET	S
11-Aug-20	NWL	3	6.200	SUMMER	32166	3RS ET	S
12-Aug-20	NEL	2	16.500	SUMMER	32166	3RS ET	P
12-Aug-20	NEL	3	19.360	SUMMER	32166	3RS ET	P
12-Aug-20	NEL	4	1.500	SUMMER	32166	3RS ET	P
12-Aug-20	NEL	2	5.270	SUMMER	32166	3RS ET	S
12-Aug-20	NEL	3	4.770	SUMMER	32166	3RS ET	S
17-Aug-20	AW	2	1.860	SUMMER	32166	3RS ET	P
17-Aug-20	AW	3	3.020	SUMMER	32166	3RS ET	P
17-Aug-20	WL	2	0.520	SUMMER	32166	3RS ET	P
17-Aug-20	WL	3	17.310	SUMMER	32166	3RS ET	P
17-Aug-20	WL	4	1.510	SUMMER	32166	3RS ET	P
17-Aug-20	WL	2	4.080	SUMMER	32166	3RS ET	S
17-Aug-20	WL	3	4.590	SUMMER	32166	3RS ET	S
17-Aug-20	WL	4	0.717	SUMMER	32166	3RS ET	S
18-Aug-20	NEL	2	29.590	SUMMER	32166	3RS ET	P
18-Aug-20	NEL	3	7.650	SUMMER	32166	3RS ET	P
18-Aug-20	NEL	2	9.100	SUMMER	32166	3RS ET	S
18-Aug-20	NEL	3	0.860	SUMMER	32166	3RS ET	S
24-Aug-20	SWL	2	35.344	SUMMER	32166	3RS ET	P
24-Aug-20	SWL	3	19.010	SUMMER	32166	3RS ET	P
24-Aug-20	SWL	2	11.416	SUMMER	32166	3RS ET	S
24-Aug-20	SWL	3	4.500	SUMMER	32166	3RS ET	S
26-Aug-20	NWL	2	13.100	SUMMER	32166	3RS ET	P
26-Aug-20	NWL	3	31.500	SUMMER	32166	3RS ET	P
26-Aug-20	NWL	4	16.400	SUMMER	32166	3RS ET	P
26-Aug-20	NWL	5	2.300	SUMMER	32166	3RS ET	P
26-Aug-20	NWL	2	4.200	SUMMER	32166	3RS ET	S
26-Aug-20	NWL	3	6.300	SUMMER	32166	3RS ET	S
26-Aug-20	NWL	4	1.000	SUMMER	32166	3RS ET	S
4-Sep-20	SWL	2	25.320	AUTUMN	32166	3RS ET	P
4-Sep-20	SWL	3	29.549	AUTUMN	32166	3RS ET	P
4-Sep-20	SWL	2	8.590	AUTUMN	32166	3RS ET	S
4-Sep-20	SWL	3	6.451	AUTUMN	32166	3RS ET	S
7-Sep-20	SWL	2	25.950	AUTUMN	32166	3RS ET	P
7-Sep-20	SWL	3	28.860	AUTUMN	32166	3RS ET	P
7-Sep-20	SWL	2	12.590	AUTUMN	32166	3RS ET	S
7-Sep-20	SWL	3	3.400	AUTUMN	32166	3RS ET	S
8-Sep-20	NWL	2	41.020	AUTUMN	32166	3RS ET	P

DATE	AREA	BEAU	KM SEARCHED	SEASON	VESSEL	TYPE	P/S
8-Sep-20	NWL	3	21.980	AUTUMN	32166	3RS ET	P
8-Sep-20	NWL	2	7.700	AUTUMN	32166	3RS ET	S
8-Sep-20	NWL	3	4.200	AUTUMN	32166	3RS ET	S
9-Sep-20	AW	2	4.940	AUTUMN	32166	3RS ET	P
9-Sep-20	WL	1	1.240	AUTUMN	32166	3RS ET	P
9-Sep-20	WL	2	12.810	AUTUMN	32166	3RS ET	P
9-Sep-20	WL	3	5.833	AUTUMN	32166	3RS ET	P
9-Sep-20	WL	2	7.540	AUTUMN	32166	3RS ET	S
9-Sep-20	WL	3	3.077	AUTUMN	32166	3RS ET	S
14-Sep-20	NWL	1	0.600	AUTUMN	32166	3RS ET	P
14-Sep-20	NWL	2	20.910	AUTUMN	32166	3RS ET	P
14-Sep-20	NWL	3	29.290	AUTUMN	32166	3RS ET	P
14-Sep-20	NWL	4	12.600	AUTUMN	32166	3RS ET	P
14-Sep-20	NWL	2	4.100	AUTUMN	32166	3RS ET	S
14-Sep-20	NWL	3	5.700	AUTUMN	32166	3RS ET	S
14-Sep-20	NWL	4	1.900	AUTUMN	32166	3RS ET	S
15-Sep-20	AW	2	3.010	AUTUMN	32166	3RS ET	P
15-Sep-20	AW	3	1.940	AUTUMN	32166	3RS ET	P
15-Sep-20	WL	2	9.663	AUTUMN	32166	3RS ET	P
15-Sep-20	WL	3	9.010	AUTUMN	32166	3RS ET	P
15-Sep-20	WL	4	0.900	AUTUMN	32166	3RS ET	P
15-Sep-20	WL	2	5.657	AUTUMN	32166	3RS ET	S
15-Sep-20	WL	3	5.440	AUTUMN	32166	3RS ET	S
17-Sep-20	NEL	2	7.670	AUTUMN	32166	3RS ET	P
17-Sep-20	NEL	3	19.980	AUTUMN	32166	3RS ET	P
17-Sep-20	NEL	4	9.600	AUTUMN	32166	3RS ET	P
17-Sep-20	NEL	2	2.050	AUTUMN	32166	3RS ET	S
17-Sep-20	NEL	3	5.500	AUTUMN	32166	3RS ET	S
17-Sep-20	NEL	4	3.100	AUTUMN	32166	3RS ET	S
22-Sep-20	NEL	2	4.100	AUTUMN	32166	3RS ET	P
22-Sep-20	NEL	3	28.500	AUTUMN	32166	3RS ET	P
22-Sep-20	NEL	4	5.000	AUTUMN	32166	3RS ET	P
22-Sep-20	NEL	2	2.800	AUTUMN	32166	3RS ET	S
22-Sep-20	NEL	3	6.900	AUTUMN	32166	3RS ET	S
22-Sep-20	NEL	4	0.300	AUTUMN	32166	3RS ET	S

CWD Small Vessel Line-transect Survey

Sighting Data

DATE	STG #	TIME	CWD/FP	GP SZ	AREA	BEAU	PSD	EFFORT	TYPE	DEC LAT	DEC LON	SEASON	BOAT ASSOC.	P/S
6-Jul-20	1	1037	CWD	1	WL	3	326	ON	3RS ET	22.2643	113.8574	SUMMER	NONE	S
6-Jul-20	2	1111	CWD	11	WL	4	634	ON	3RS ET	22.2468	113.8514	SUMMER	NONE	S
6-Jul-20	3	1216	CWD	1	WL	3	284	ON	3RS ET	22.2120	113.8363	SUMMER	NONE	P
6-Jul-20	4	1245	CWD	2	WL	3	329	ON	3RS ET	22.1961	113.8400	SUMMER	NONE	P
13-Jul-20	1	1033	CWD	5	WL	2	1238	ON	3RS ET	22.2672	113.8600	SUMMER	NONE	S
13-Jul-20	2	1126	CWD	4	WL	3	601	ON	3RS ET	22.2416	113.8299	SUMMER	NONE	P
13-Jul-20	3	1144	CWD	5	WL	3	1020	ON	3RS ET	22.2377	113.8266	SUMMER	NONE	S
13-Jul-20	4	1203	CWD	1	WL	3	13	ON	3RS ET	22.2235	113.8242	SUMMER	NONE	P
13-Jul-20	5	1223	CWD	4	WL	3	322	ON	3RS ET	22.2142	113.8266	SUMMER	NONE	P
13-Jul-20	6	1304	CWD	18	WL	3	211	ON	3RS ET	22.2020	113.8240	SUMMER	NONE	S
13-Jul-20	7	1338	CWD	19	WL	3	221	ON	3RS ET	22.1962	113.8332	SUMMER	NONE	P
13-Jul-20	8	1410	CWD	4	WL	3	129	ON	3RS ET	22.1910	113.8419	SUMMER	NONE	S
20-Jul-20	1	1028	CWD	1	SWL	2	171	ON	3RS ET	22.2119	113.9359	SUMMER	NONE	P
20-Jul-20	2	1237	CWD	1	SWL	2	268	ON	3RS ET	22.1767	113.9072	SUMMER	NONE	S
20-Jul-20	3	1249	CWD	4	SWL	2	362	ON	3RS ET	22.1845	113.9046	SUMMER	NONE	S
20-Jul-20	4	1407	CWD	9	SWL	3	255	ON	3RS ET	22.1673	113.8883	SUMMER	NONE	P
20-Jul-20	5	1424	CWD	5	SWL	2	243	ON	3RS ET	22.1776	113.8883	SUMMER	NONE	P
20-Jul-20	6	1510	CWD	1	SWL	2	130	ON	3RS ET	22.1765	113.8784	SUMMER	NONE	P
20-Jul-20	7	1532	CWD	5	SWL	3	247	ON	3RS ET	22.1682	113.8685	SUMMER	NONE	P
20-Jul-20	8	1604	CWD	3	SWL	2	51	ON	3RS ET	22.1962	113.8586	SUMMER	NONE	P
20-Jul-20	9	1640	CWD	2	SWL	2	42	ON	3RS ET	22.1921	113.8494	SUMMER	NONE	P
21-Jul-20	1	1054	FP	7	SWL	1	146	ON	3RS ET	22.1486	113.9340	SUMMER	NONE	S
21-Jul-20	2	1255	CWD	3	SWL	3	46	ON	3RS ET	22.1928	113.8977	SUMMER	NONE	P
21-Jul-20	3	1410	CWD	2	SWL	2	161	ON	3RS ET	22.1915	113.8790	SUMMER	NONE	P
21-Jul-20	4	1426	CWD	3	SWL	2	241	ON	3RS ET	22.1723	113.8788	SUMMER	NONE	P
21-Jul-20	5	1511	CWD	4	SWL	2	21	ON	3RS ET	22.1962	113.8587	SUMMER	NONE	P
21-Jul-20	6	1537	CWD	1	SWL	2	524	ON	3RS ET	22.1700	113.8560	SUMMER	NONE	S
21-Jul-20	7	1551	CWD	3	SWL	3	188	ON	3RS ET	22.1862	113.8493	SUMMER	PURSE SEINER	P
22-Jul-20	1	1202	CWD	2	NWL	2	308	ON	3RS ET	22.3963	113.8876	SUMMER	NONE	P
7-Aug-20	1	1006	CWD	1	WL	2	57	ON	3RS ET	22.2972	113.8611	SUMMER	NONE	P
7-Aug-20	2	1033	CWD	2	WL	2	96	ON	3RS ET	22.2768	113.8514	SUMMER	NONE	S

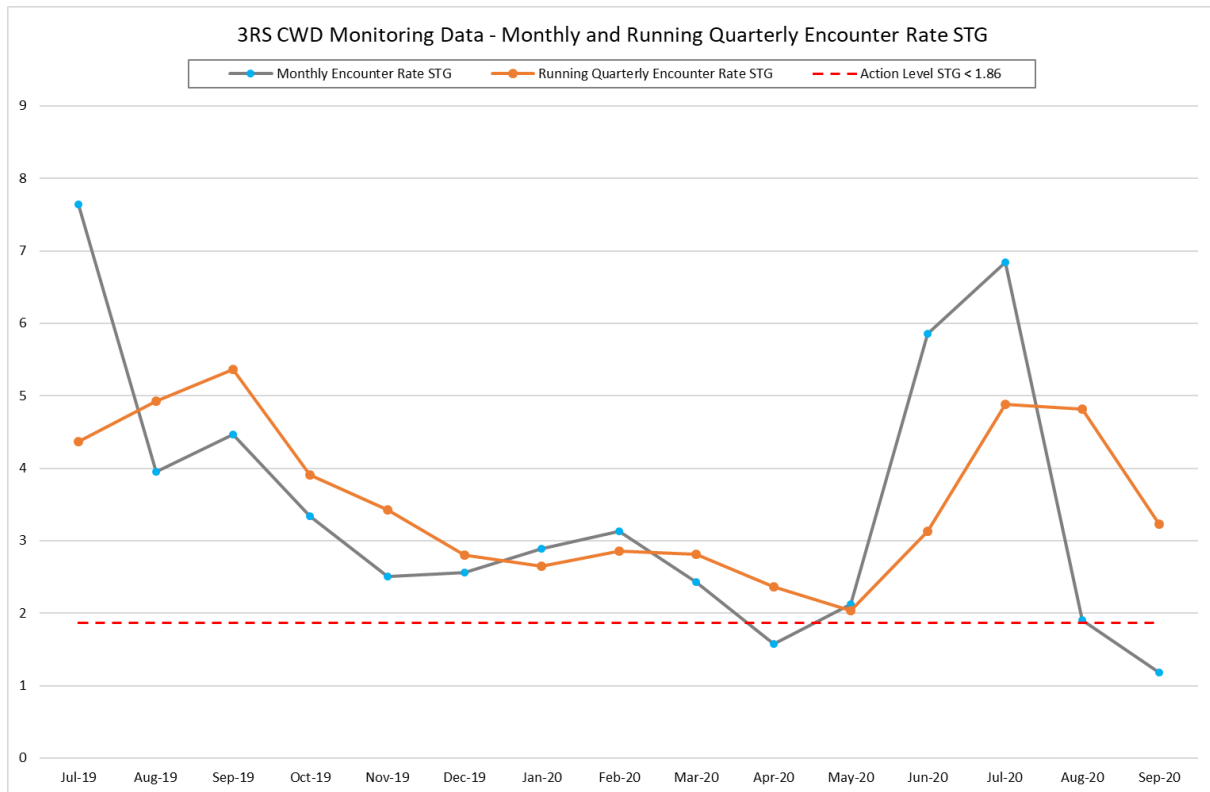
DATE	STG #	TIME	CWD/FP	GP SZ	AREA	BEAU	PSD	EFFORT	TYPE	DEC LAT	DEC LON	SEASON	BOAT ASSOC.	P/S
7-Aug-20	3	1158	CWD	3	WL	3	8	ON	3RS ET	22.2174	113.8200	SUMMER	NONE	S
7-Aug-20	4	1228	CWD	13	WL	3	111	ON	3RS ET	22.2140	113.8303	SUMMER	NONE	P
7-Aug-20	5	1337	CWD	1	WL	3	235	ON	3RS ET	22.1955	113.8396	SUMMER	NONE	P
10-Aug-20	1	1122	FP	4	SWL	2	59	ON	3RS ET	22.1802	113.9280	SUMMER	NONE	P
10-Aug-20	2	1515	CWD	2	SWL	3	3	ON	3RS ET	22.1883	113.8491	SUMMER	NONE	P
10-Aug-20	3	1528	CWD	1	SWL	3	37	ON	3RS ET	22.1931	113.8499	SUMMER	NONE	P
17-Aug-20	1	1102	CWD	9	WL	3	229	ON	3RS ET	22.2408	113.8378	SUMMER	NONE	P
17-Aug-20	2	1222	CWD	1	WL	4	304	ON	3RS ET	22.1928	113.8424	SUMMER	NONE	S
24-Aug-20	1	1054	FP	2	SWL	2	61	ON	3RS ET	22.1462	113.9319	SUMMER	NONE	S
24-Aug-20	2	1318	FP	8	SWL	2	63	ON	3RS ET	22.1565	113.8876	SUMMER	NONE	P
4-Sep-20	1	1111	FP	3	SWL	2	93	ON	3RS ET	22.1500	113.9273	AUTUMN	NONE	P
4-Sep-20	2	1129	FP	6	SWL	2	328	ON	3RS ET	22.1869	113.9273	AUTUMN	NONE	P
4-Sep-20	3	1225	FP	1	SWL	2	47	ON	3RS ET	22.1547	113.9040	AUTUMN	NONE	S
4-Sep-20	4	1330	FP	7	SWL	3	15	ON	3RS ET	22.1493	113.8977	AUTUMN	NONE	P
9-Sep-20	1	1030	CWD	2	WL	2	189	ON	3RS ET	22.2632	113.8568	AUTUMN	NONE	S
9-Sep-20	2	1213	CWD	8	WL	3	323	ON	3RS ET	22.1965	113.8398	AUTUMN	NONE	P
15-Sep-20	1	1053	CWD	2	WL	2	85	ON	3RS ET	22.2689	113.8508	AUTUMN	NONE	P
15-Sep-20	2	1158	CWD	2	WL	3	20	ON	3RS ET	22.2320	113.8378	AUTUMN	NONE	P
15-Sep-20	3	1242	CWD	5	WL	3	225	ON	3RS ET	22.2058	113.8398	AUTUMN	NONE	S

Abbreviations: STG# = Sighting Number; GP SZ = Group Size; BEAU = Beaufort Sea State; PSD = Perpendicular Distance (in metres); N/A = Not Applicable; DEC LAT = Latitude (WGS84 in Decimal), DEC LON = Longitude (WGS84 in Decimal); BOAT ASSOC. = Fishing Boat Association

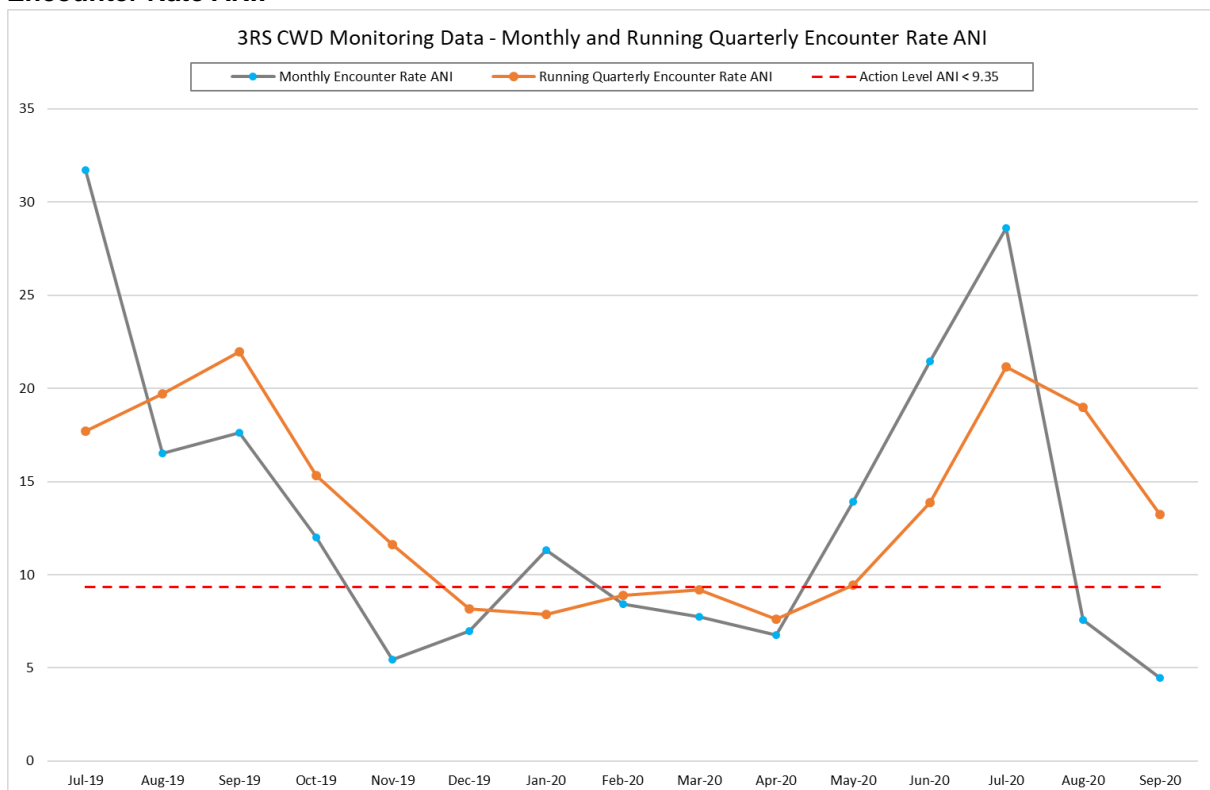
Sighting data of finless porpoise (FP) are presented for reference only. No relevant figure or text will be mentioned in the quarterly EM&A report. All FP sightings are excluded in calculation.

Graphical Presentation of Monthly and Running Quarterly Encounter Rates from July 2019 to September 2020

Encounter Rate STG:











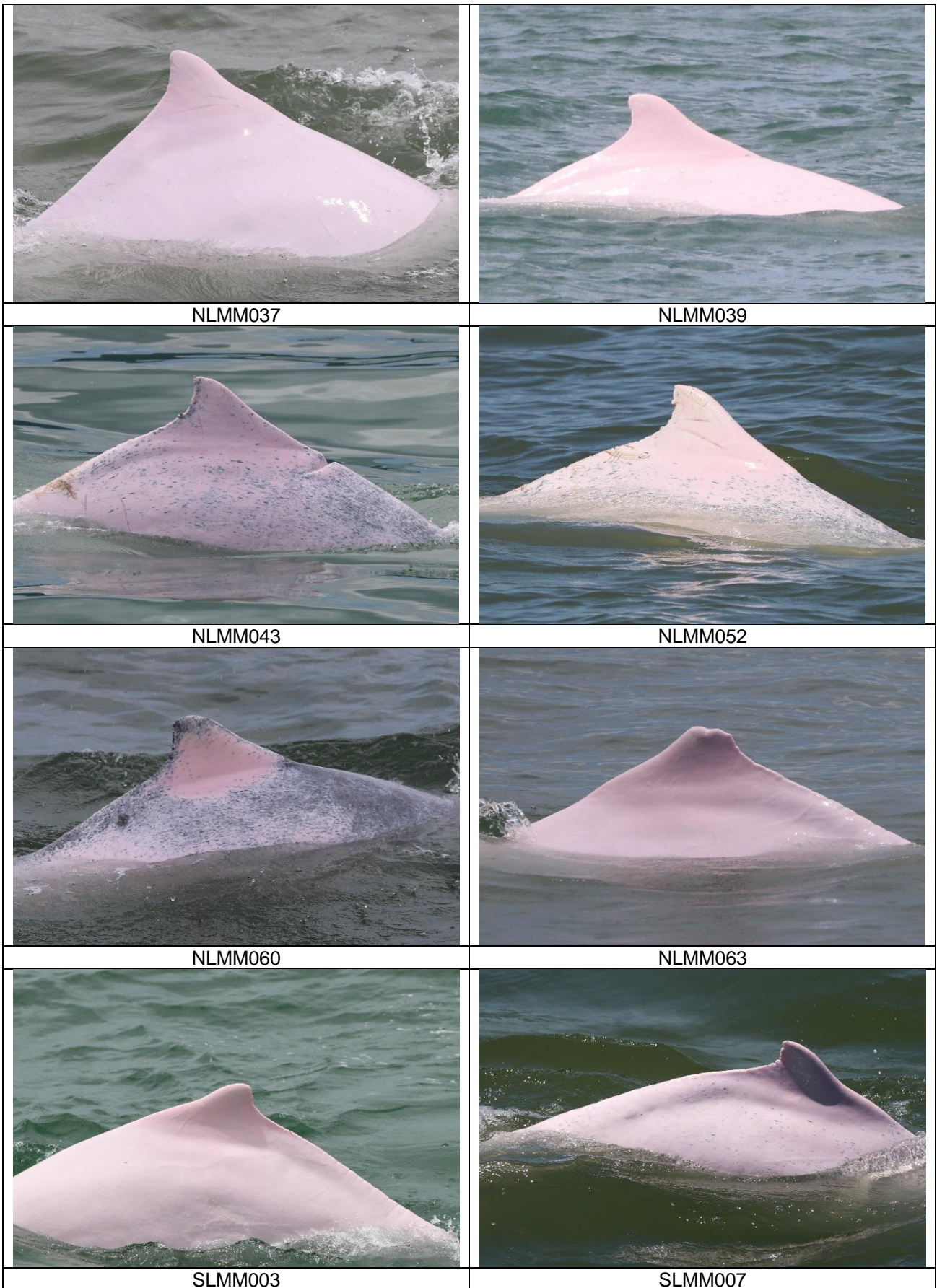
Encounter Rate ANI:

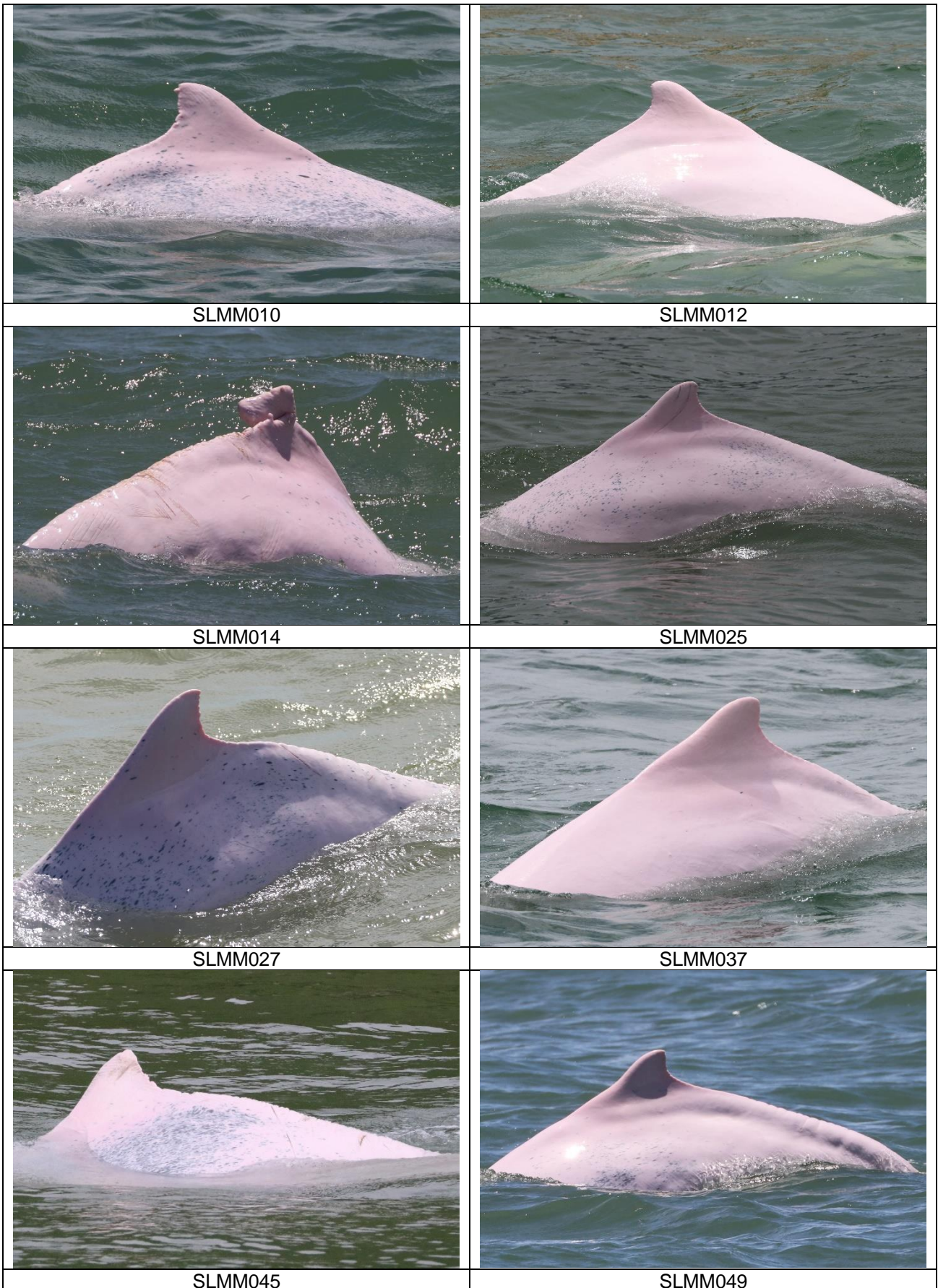


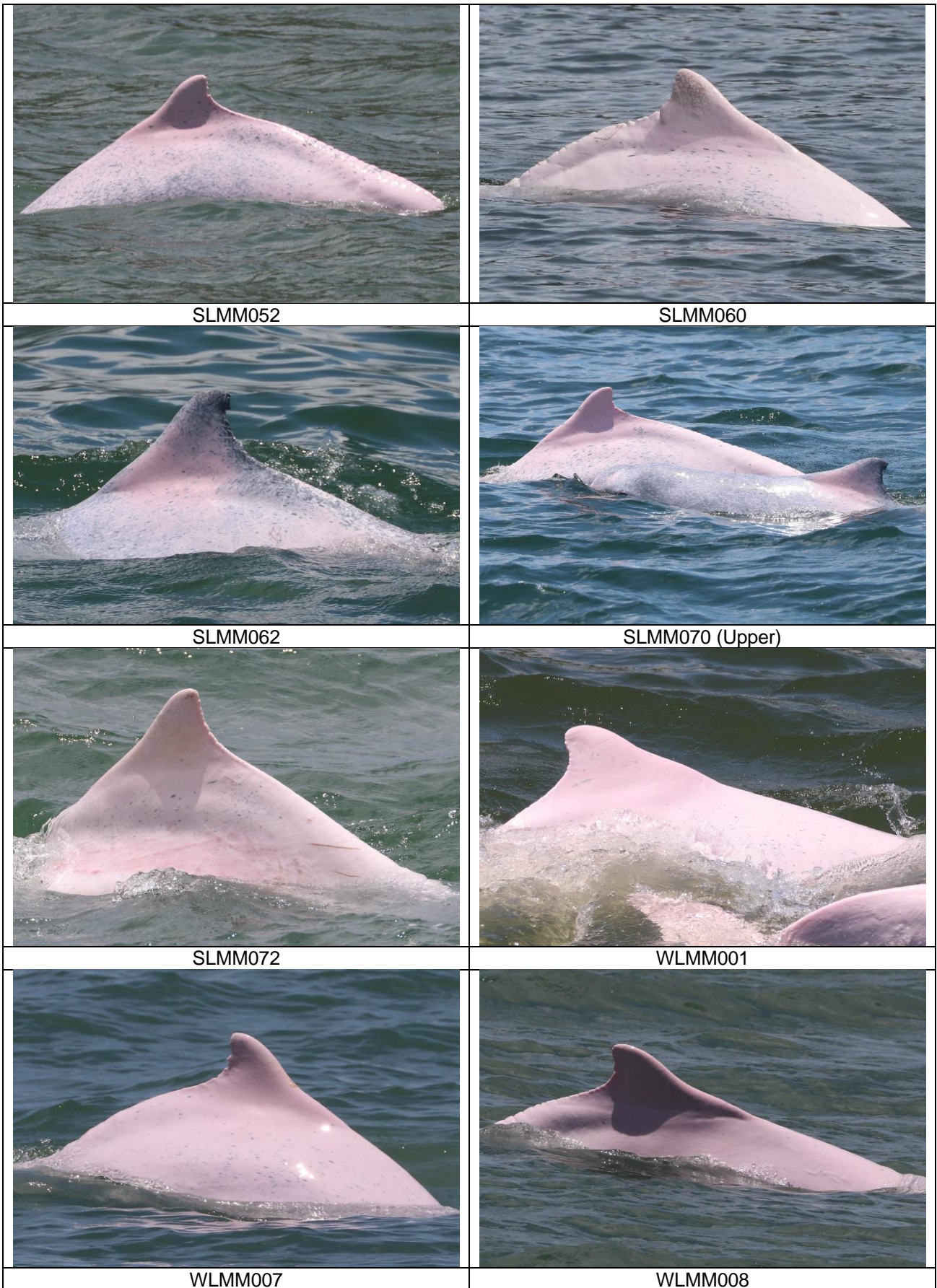
CWD Small Vessel Line-transect Survey









Photo Identification

	
NLMM012	NLMM013
	
NLMM015	NLMM019
	
NLMM020	NLMM023
	
NLMM027	NLMM034







	
WLMM009	WLMM013
	
WLMM019	WLMM028
	
WLMM029	WLMM030
	
WLMM038	WLMM043



WLMM047



WLMM056



WLMM060



WLMM062



WLMM063



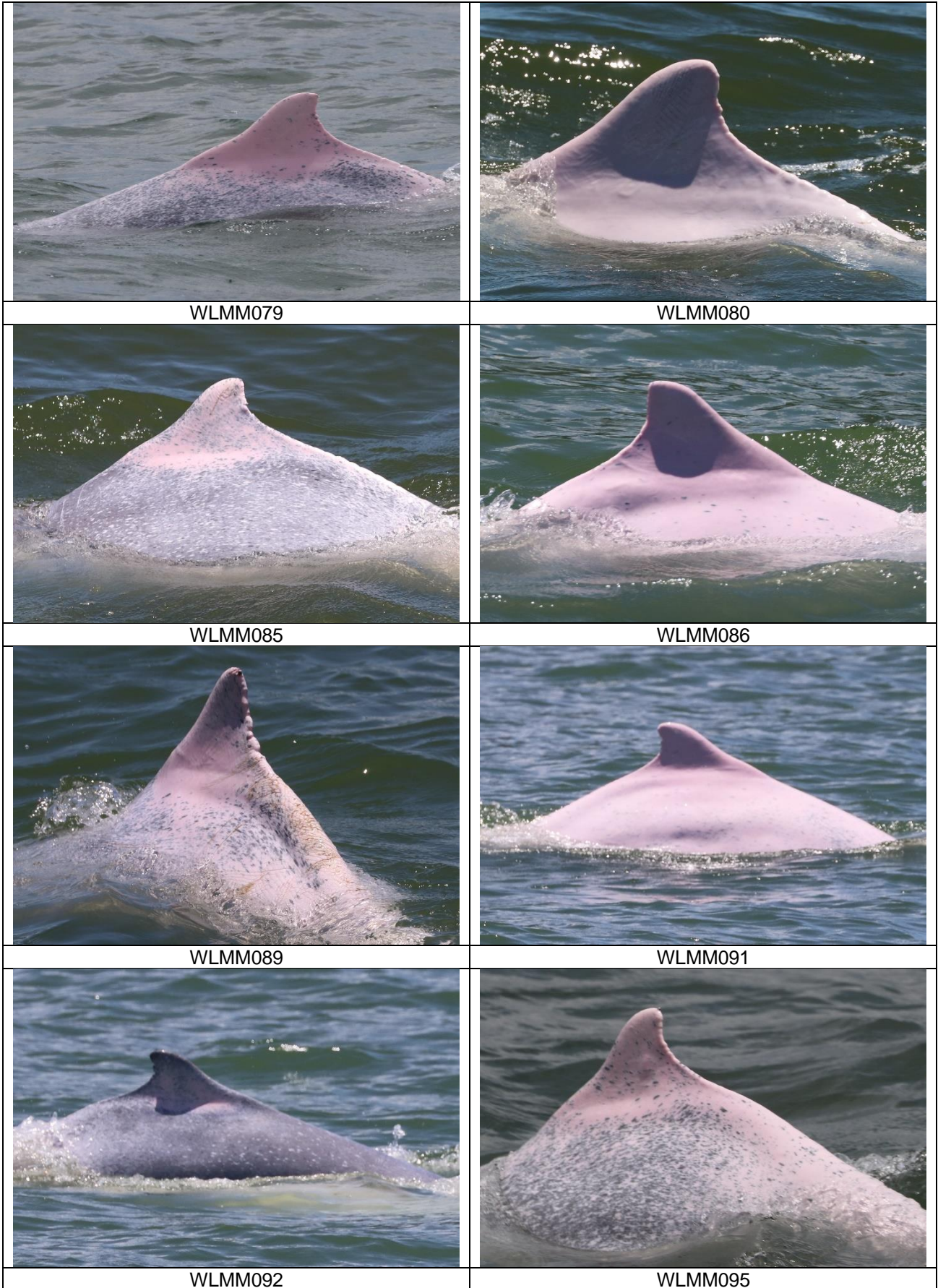
WLMM068

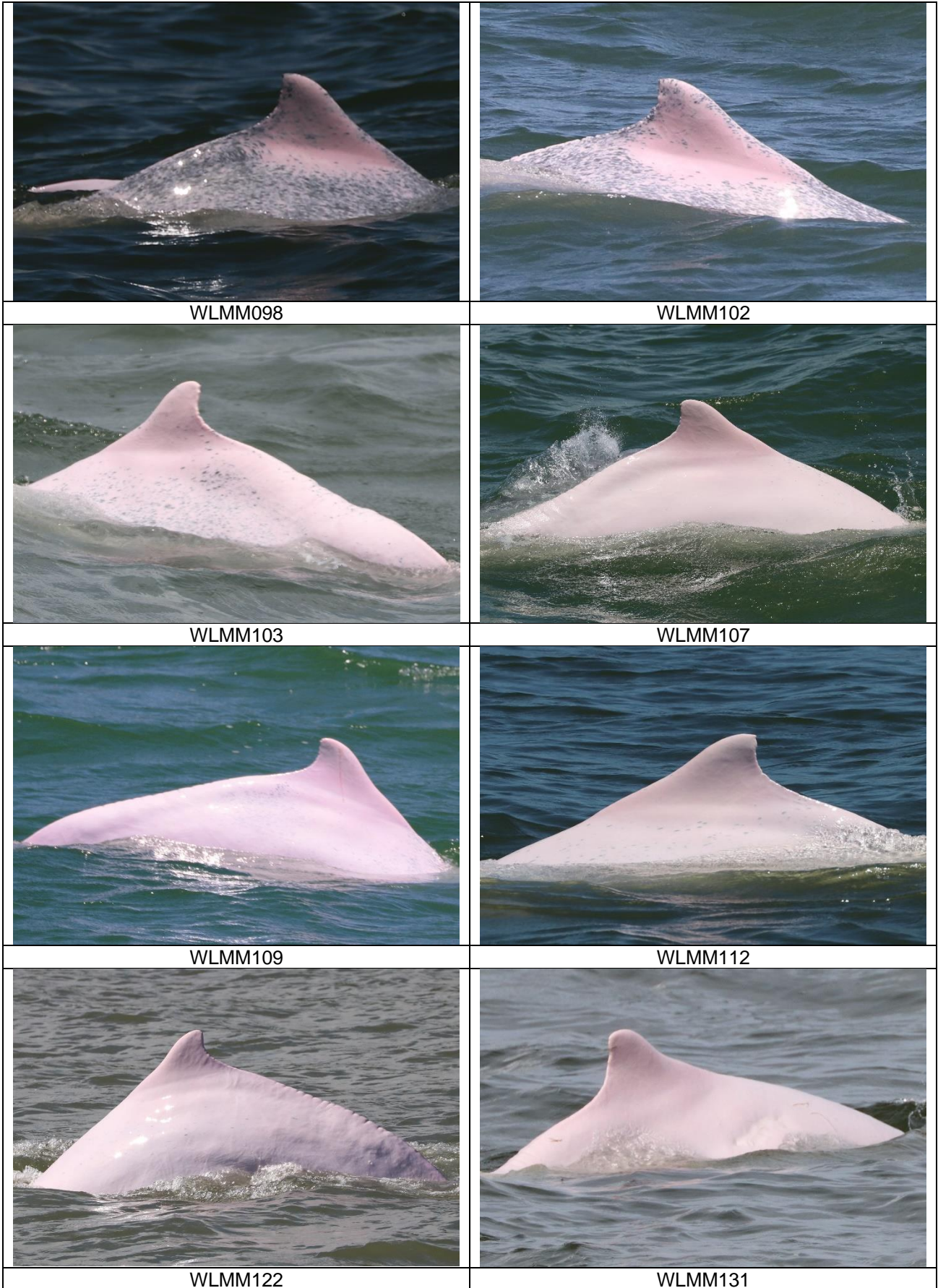


WLMM071



WLMM073







WLMM133



WLMM136



WLMM147 (Left)



WLMM149



WLMM154



WLMM155



WLMM156



WLMM157






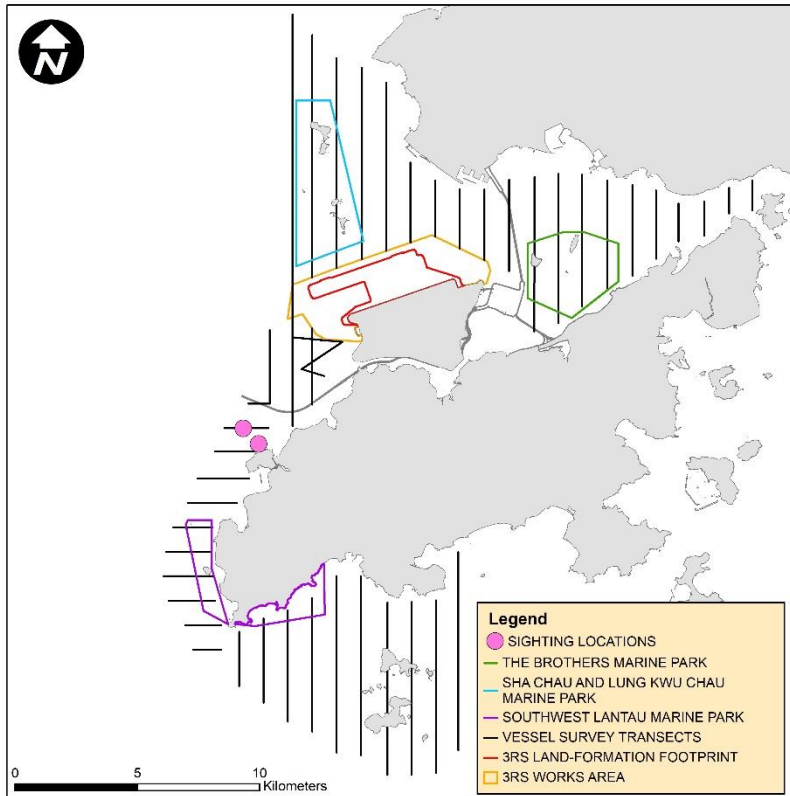
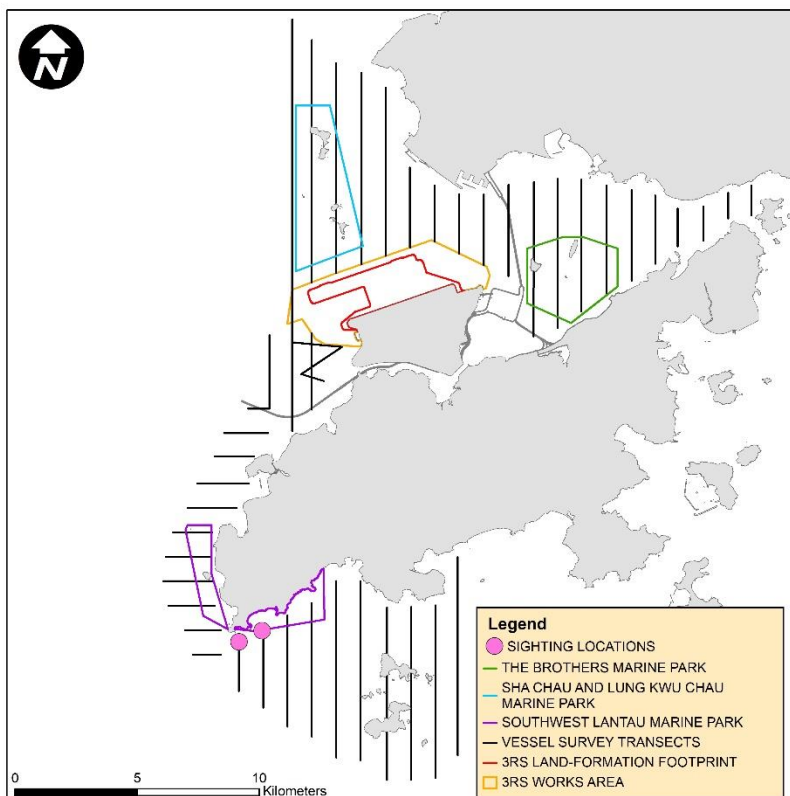
	
WLMM158	WLMM159
	
WLMM160	WLMM161
	
WLMM162	

Photo Identification – Re-sighting Locations

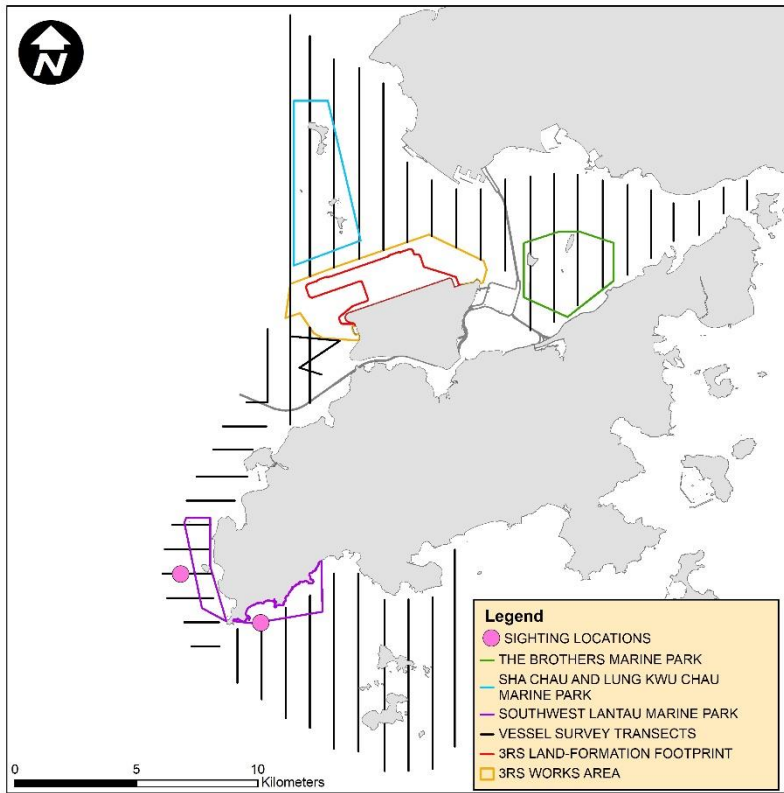
NLMM023



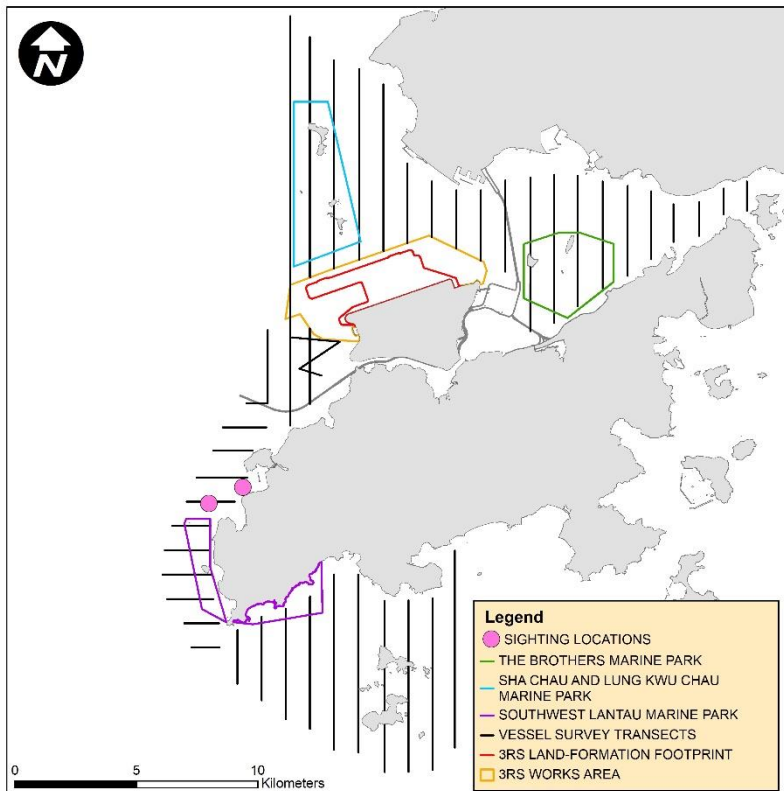
NLMM037



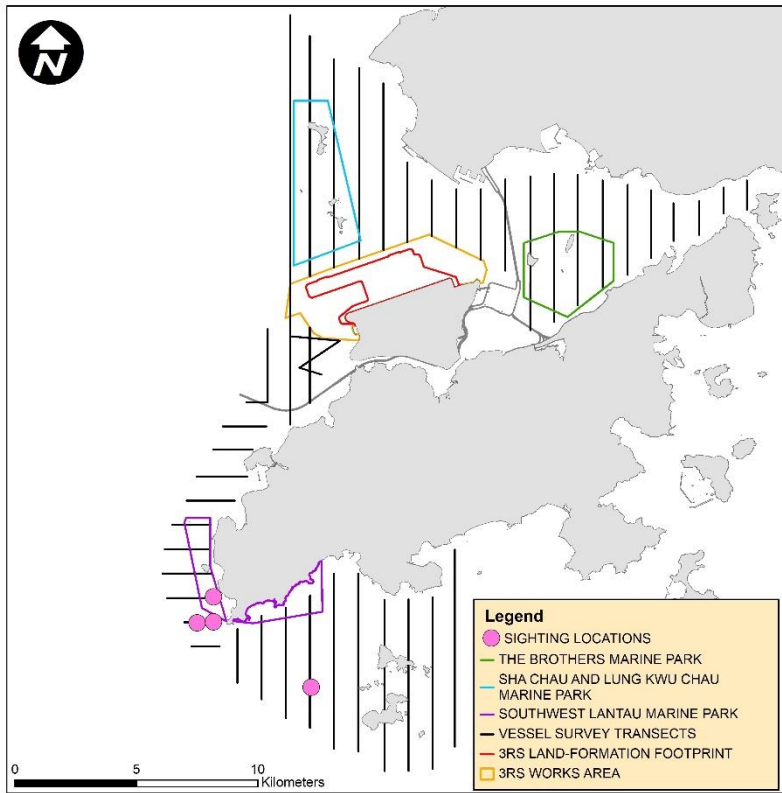
NLMM052



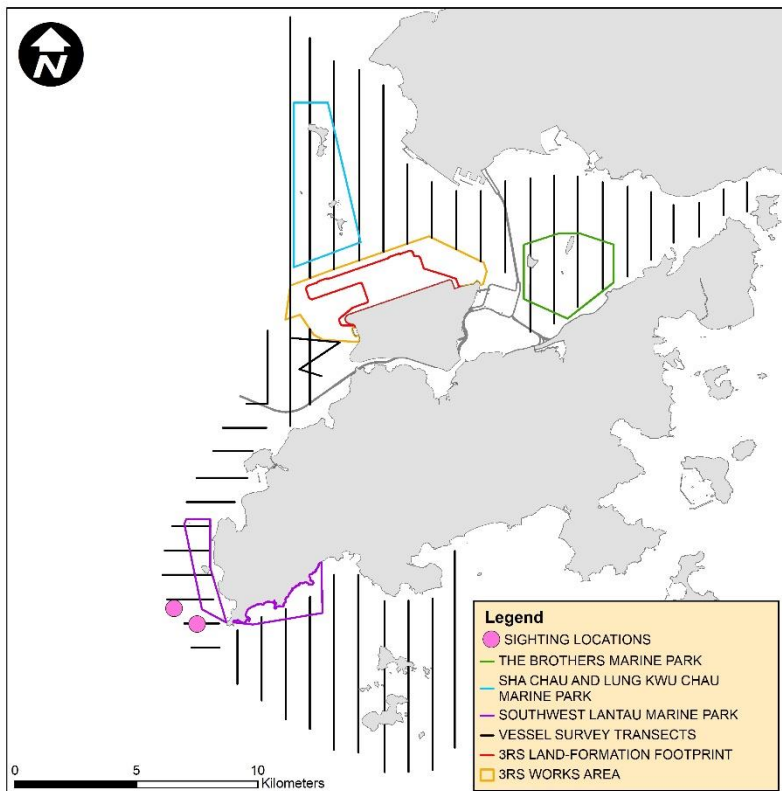
NLMM060



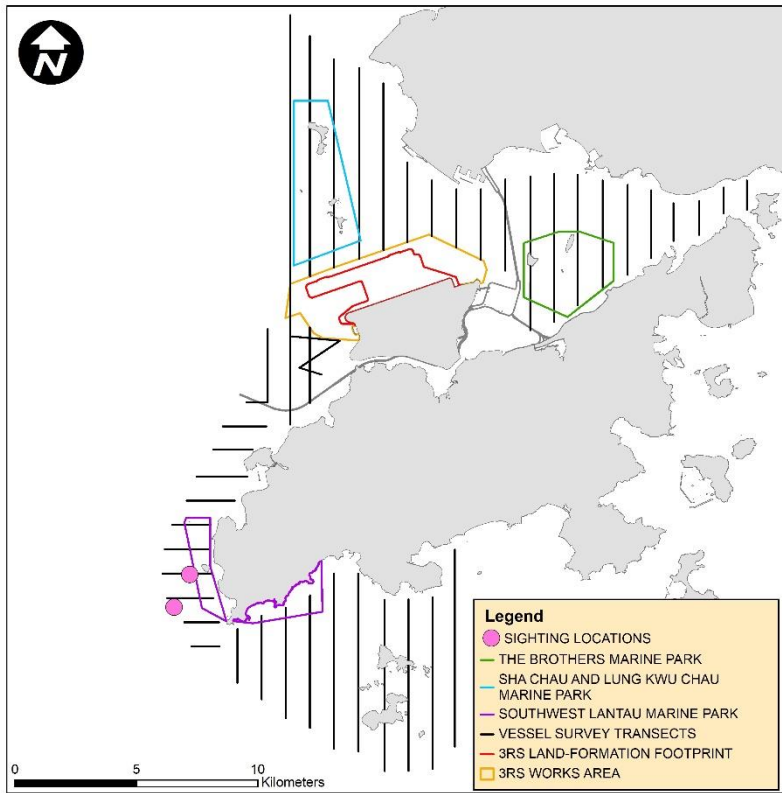
SLMM003



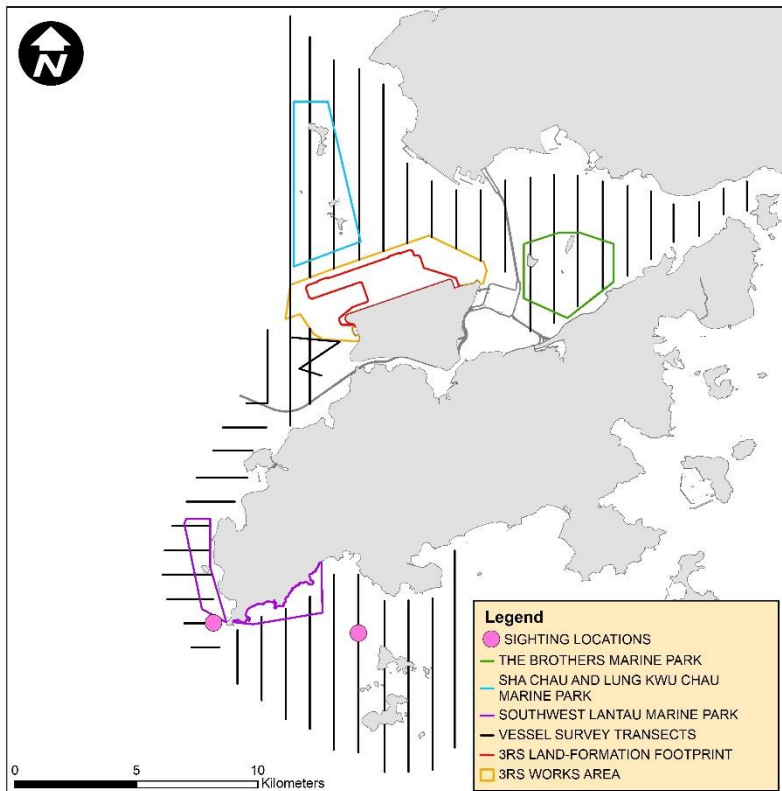
SLMM007



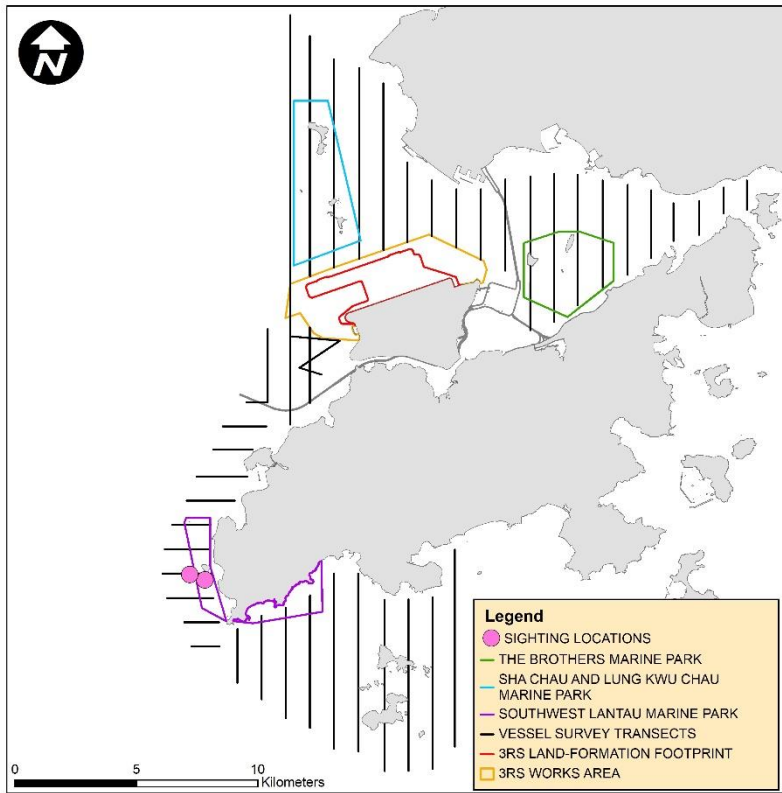
SLMM010



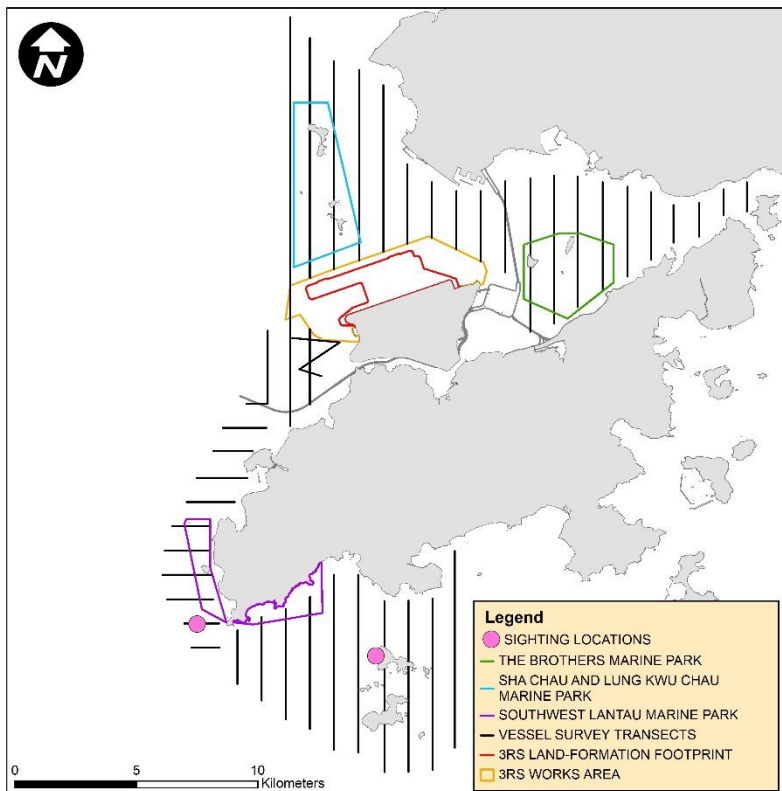
SLMM012



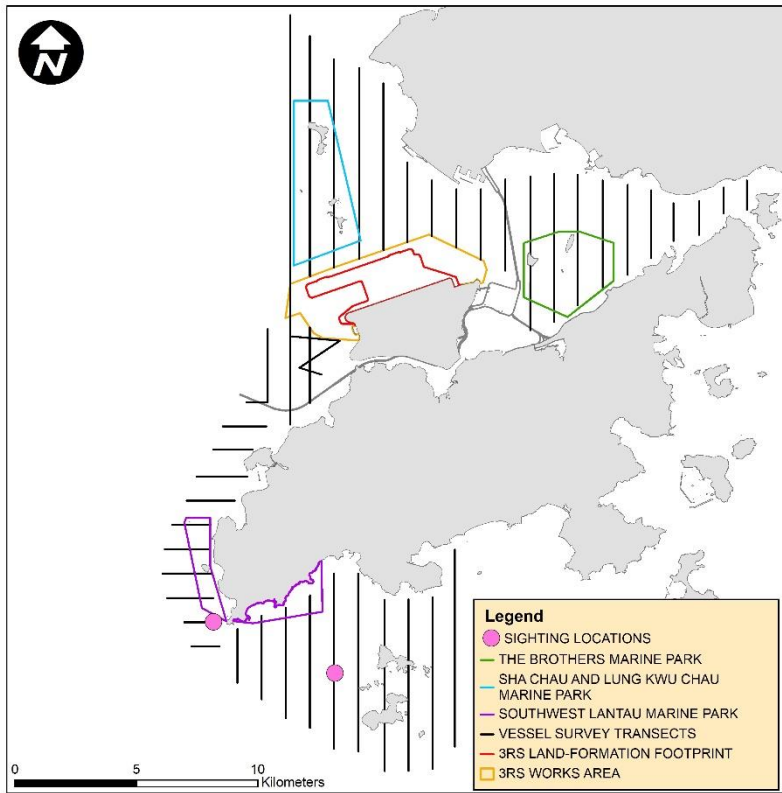
SLMM014



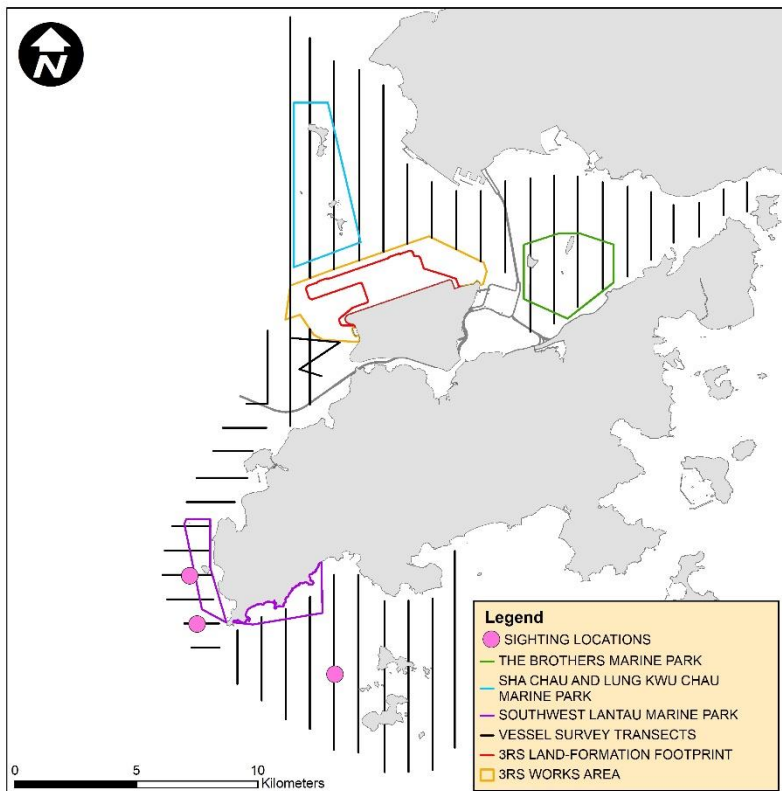
SLMM025



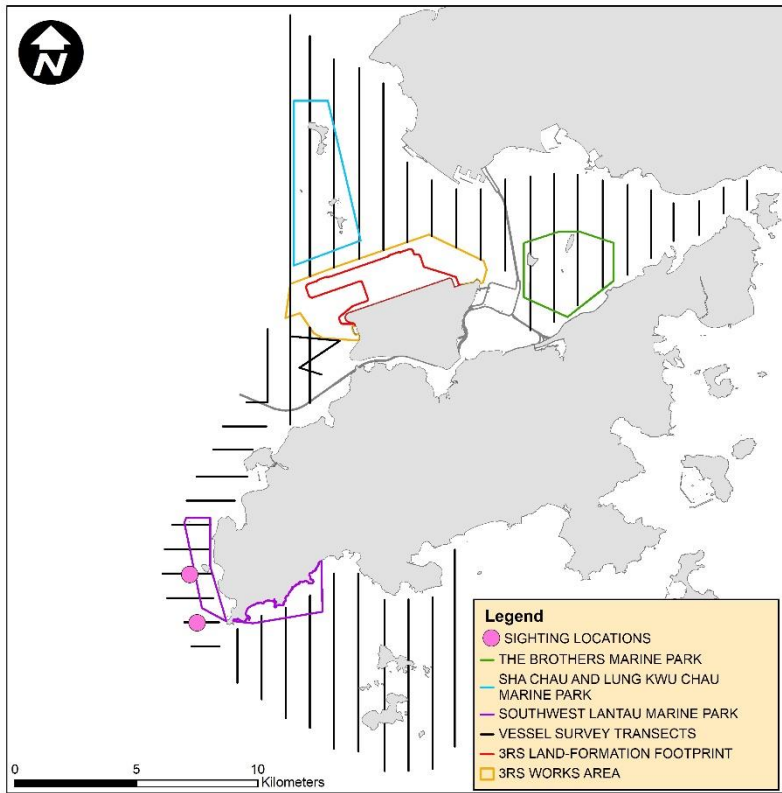
SLMM037



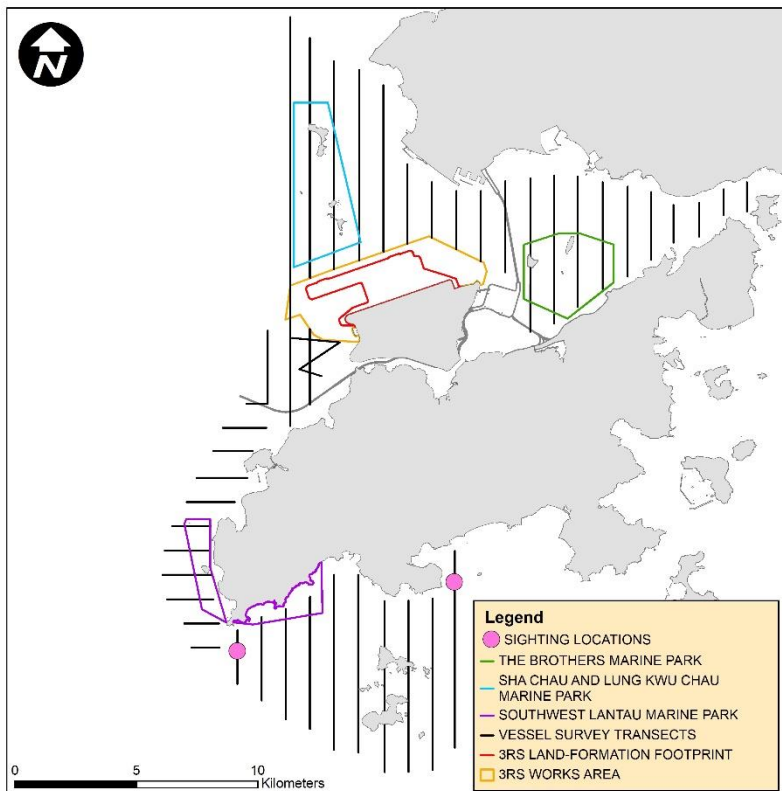
SLMM049



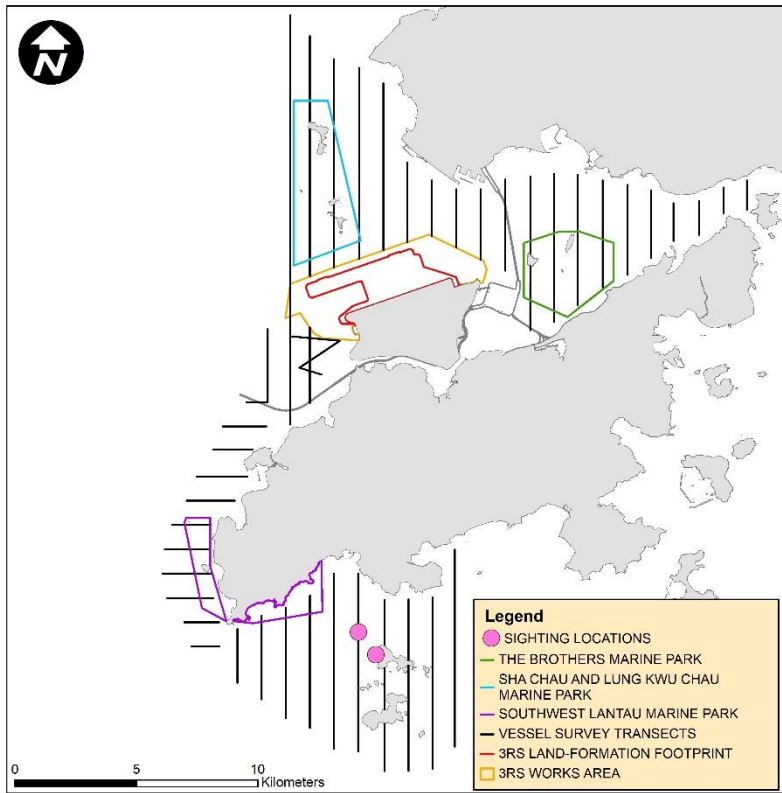
SLMM052



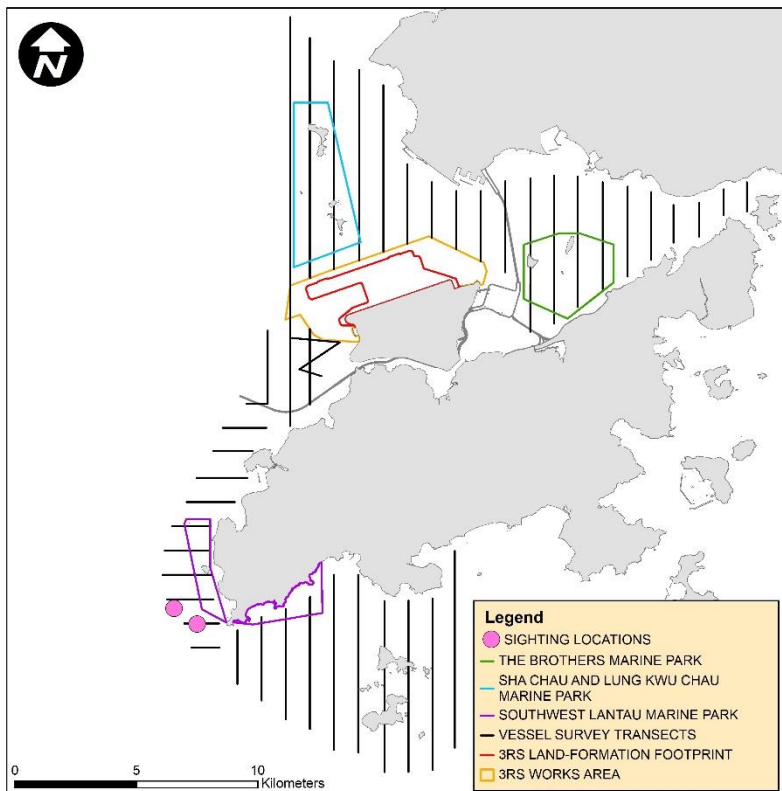
SLMM060



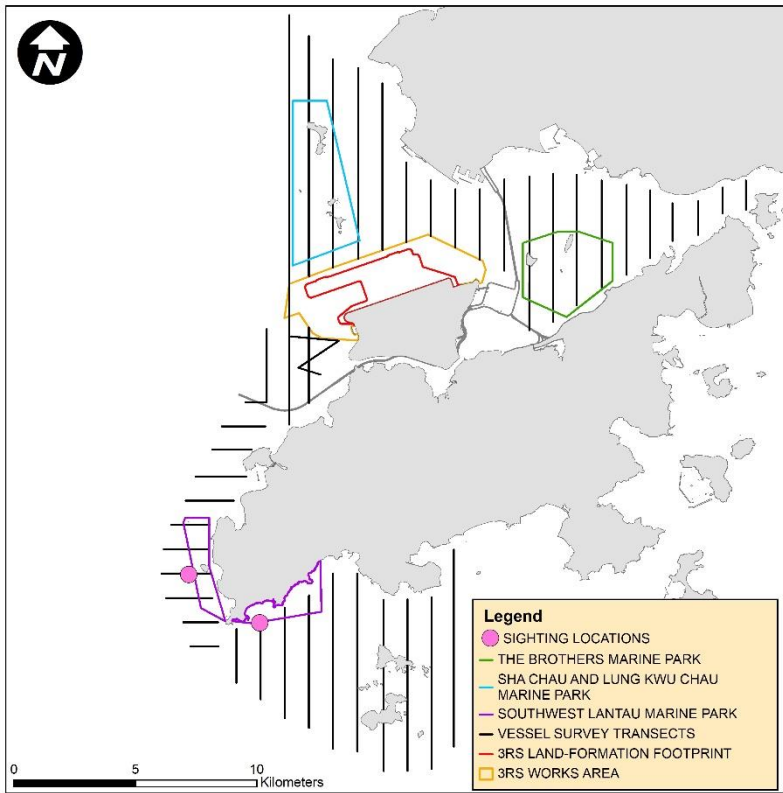
SLMM062



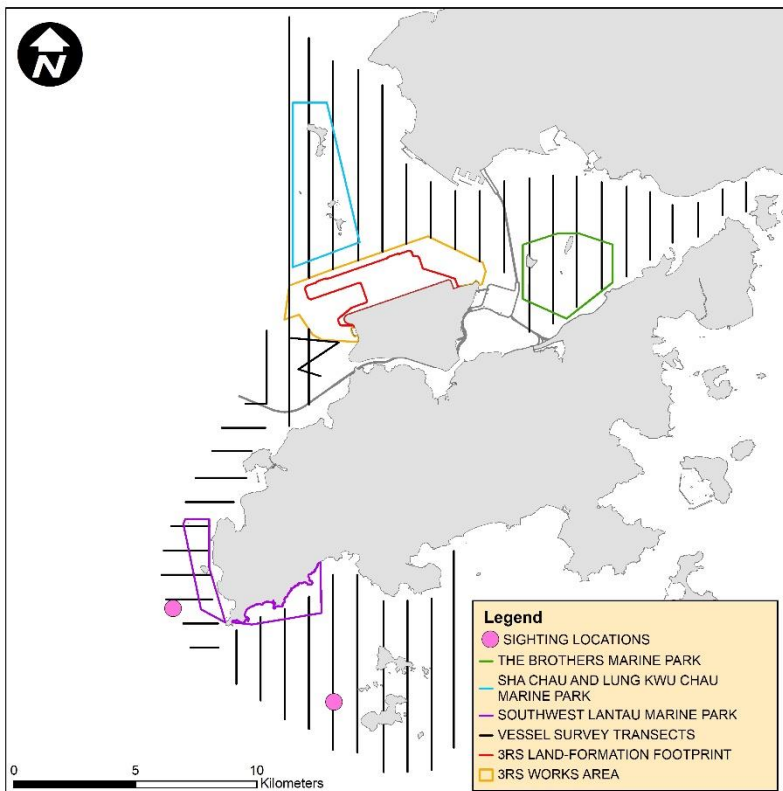
WLMM001



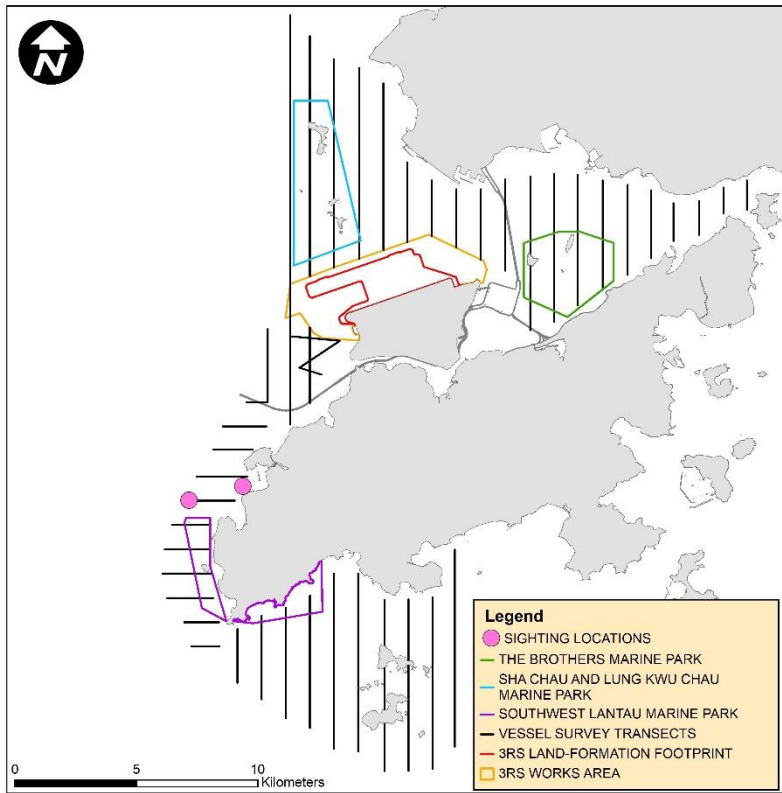
WLMM008



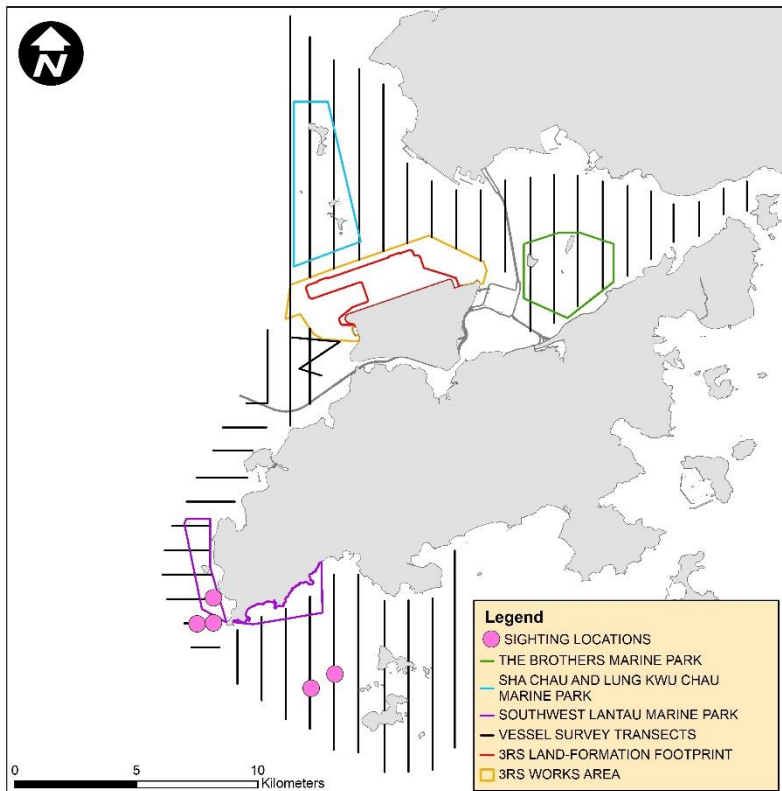
WLMM009



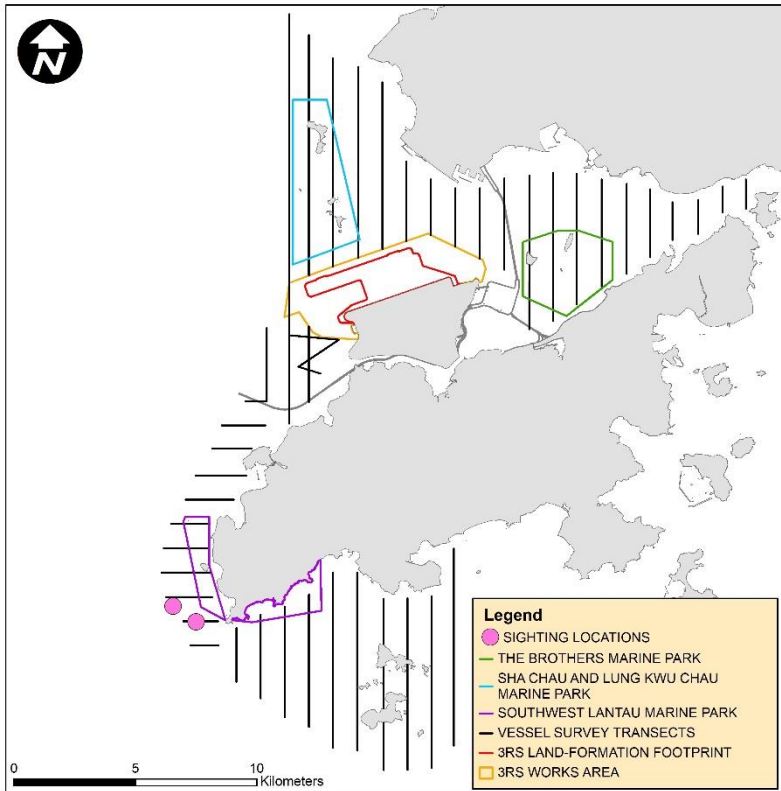
WLMM030



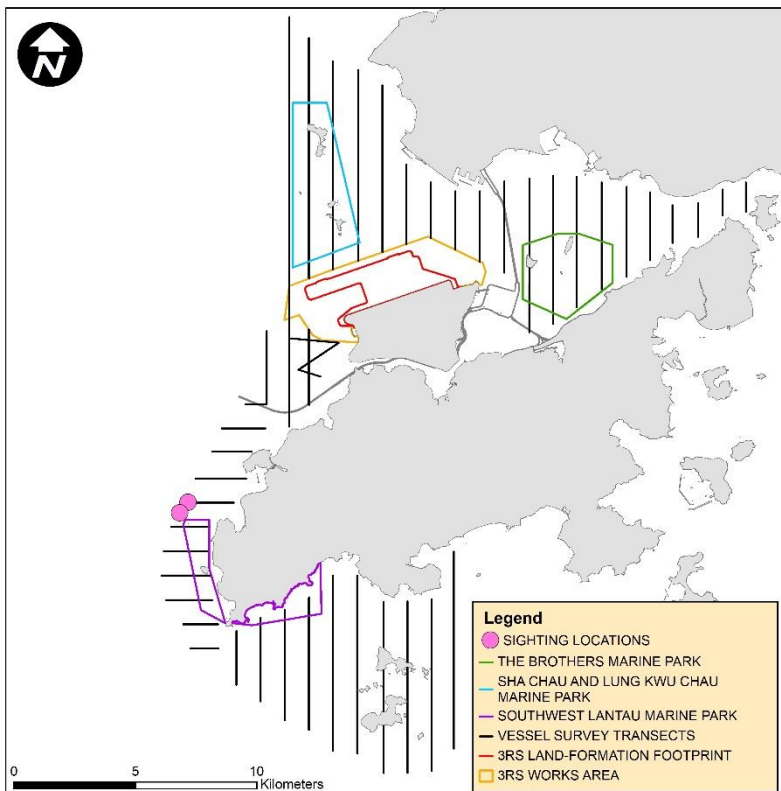
WLMM079



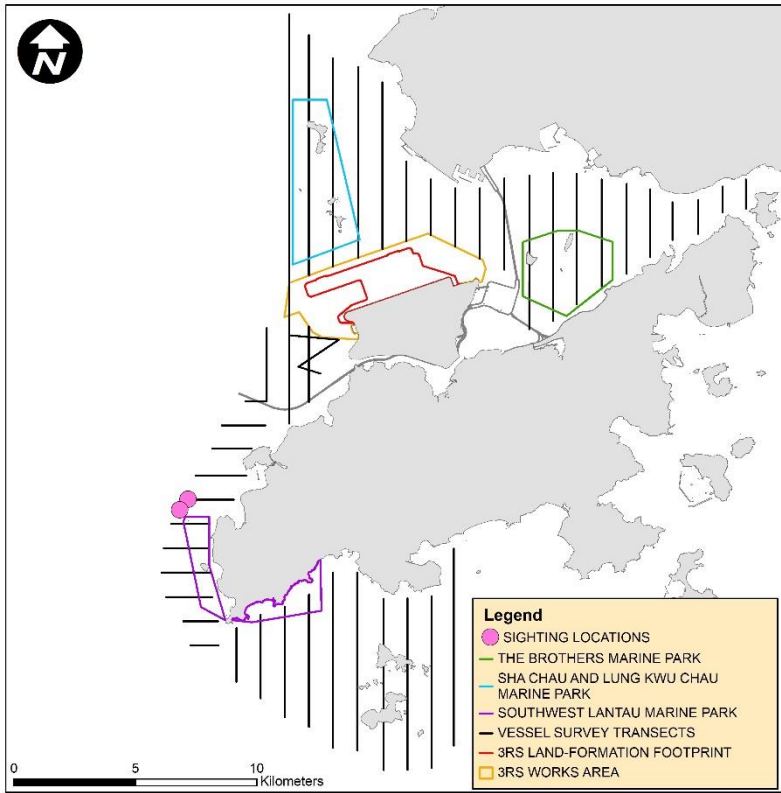
WLMM085



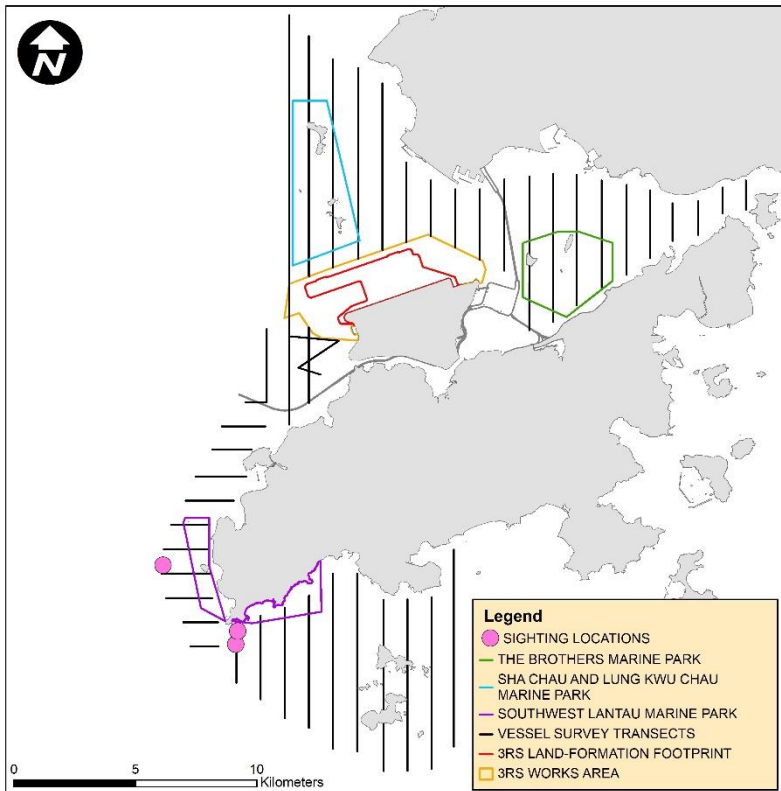
WLMM091



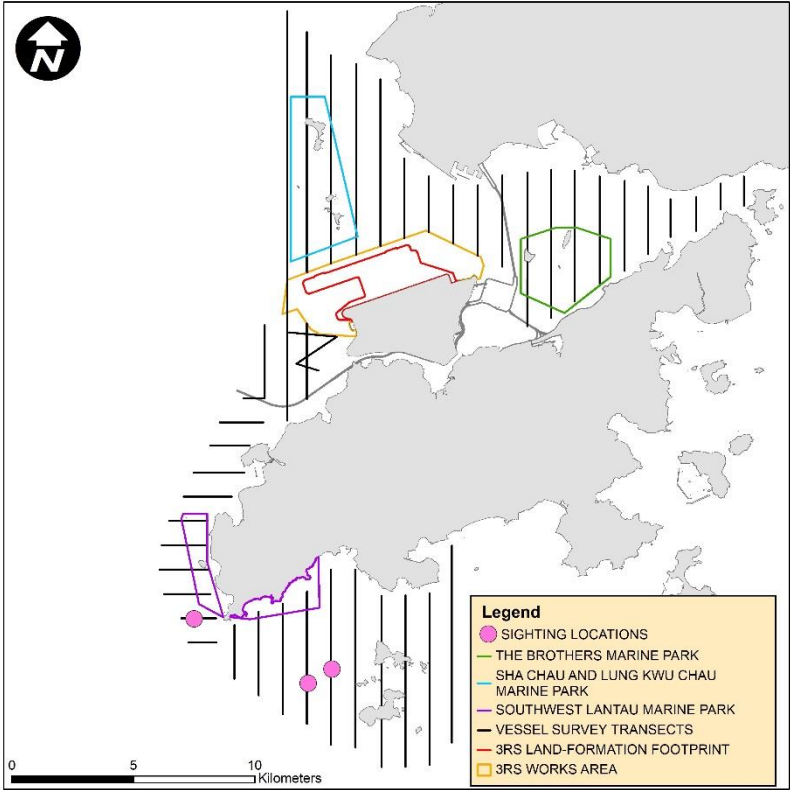
WLMM092



WLMM131



WLMM147



CWD Land-based Theodolite Tracking**CWD Groups by Survey Date**

Date	Station	Start	End	Duration	Beaufort	Visibility	No. of Focal Follow	Dolphin Group Size
7/Jul/20	Sha Chau	10:45	16:45	6:00	2-3	1	0	-
15/Jul/20	Lung Kwu Chau	8:54	14:54	6:00	2-3	1	0	-
17/Aug/20	Lung Kwu Chau	8:55	14:55	6:00	2-3	1-2	2	1-2
24/Aug/20	Sha Chau	10:56	16:56	6:00	2	2	0	-
3/Sep/20	Lung Kwu Chau	9:01	15:01	6:00	2	3	0	-
7/Sep/20	Sha Chau	10:37	16:37	6:00	2	2	0	-

Visibility: 1=Excellent, 2=Good, 3=Fair, 4=Poor

Photo Records of Retained, Transplanted and To-Be-Transplanted Trees

Photos of Retained Trees in the Reporting Period



T01



T03



T04



T07



T08



T09



T10



T11



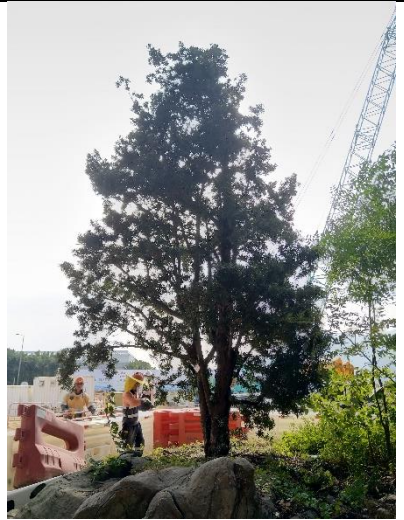
T12



T813



T833



T834



T1252



T1253



T1254



T1255



T1256



T1257



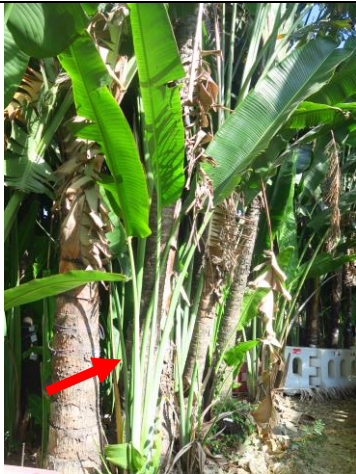
T1258



T1259



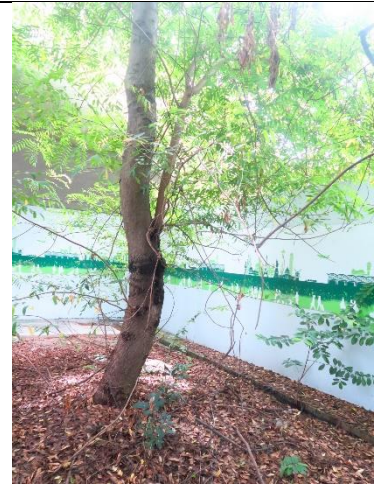
T1260



T1278



T1280



T3030



T3031



T3189



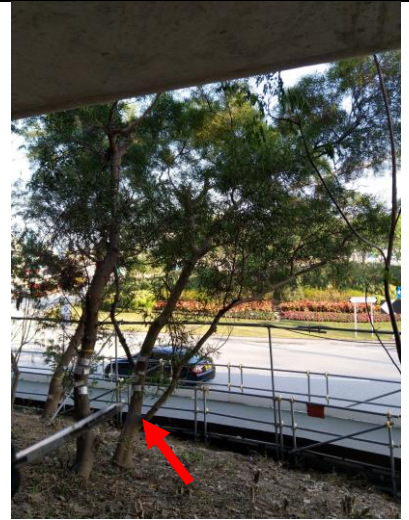
T3191



T3194



CT1375



CT1376



CT1378



CT1387



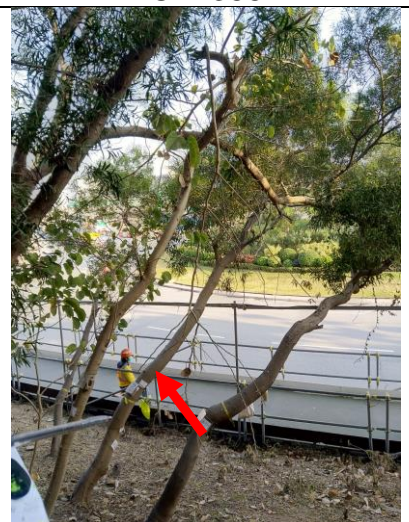
CT1388



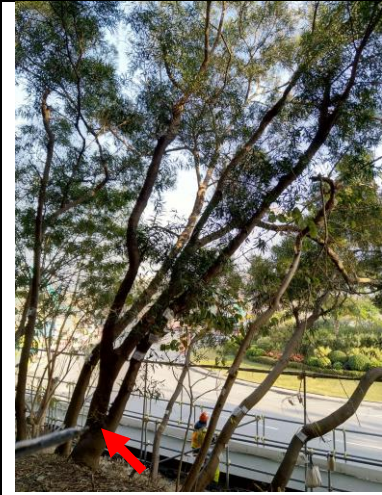
CT1389



CT1390



CT1391



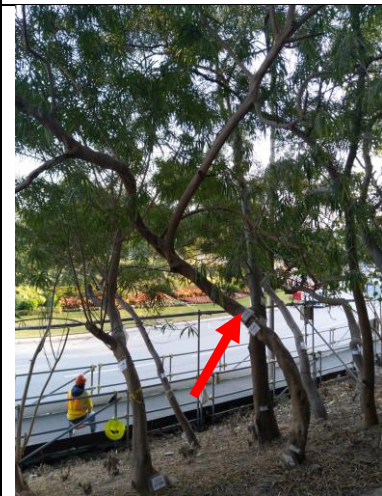
CT1392



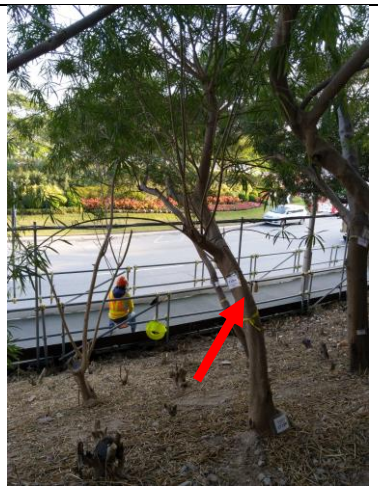
CT1393



CT1394



CT1395



CT1396



CT1397



CT1399



CT1403



CT1404



CT1406



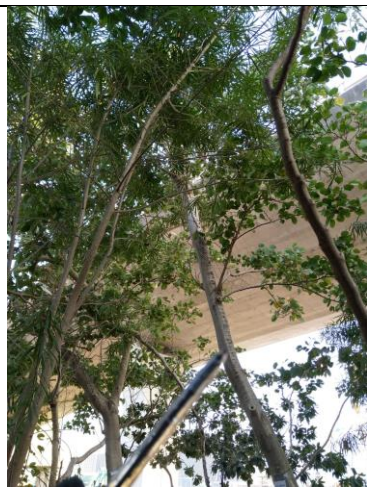
CT1411



CT1413



CT1414



CT1415



CT1416



CT1417



CT1418



CT1426



CT1427



CT1428



CT1429



CT1432



CT1433



CT1443



CT1448



CT1449



CT1450



CT1451



CT1463



CT1464



CT1466



CT1467



CT1515



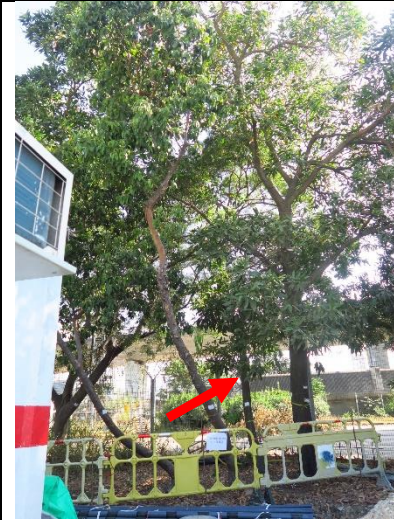
CT1516



CT1517



CT1518



CT1519



CT1520



CT1521



CT1522



CT1523



CT1524



CT1843



CT1844



CT1845



CT1846



CT1847



CT1848



CT1849



CT1850



CT1851



CT1852



CT1853



CT1855



CT1856



CT1857



CT1859



CT1860



CT1861



CT1862



CT1863



CT1865



CT1866



CT1867



CT1868



CT1869



CT1870



CT1871



CT1872



CT1873



CT1874



CT1876



CT1877



CT1878



CT1885



CT1886



CT1887



CT1888



CT1889



CT1890



CT1892



CT1894



CT1897



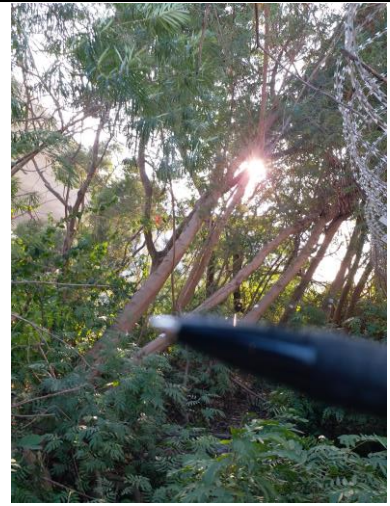
CT1898



CT1899



CT1900



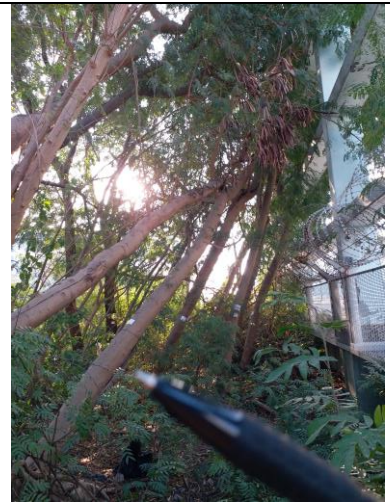
CT1901



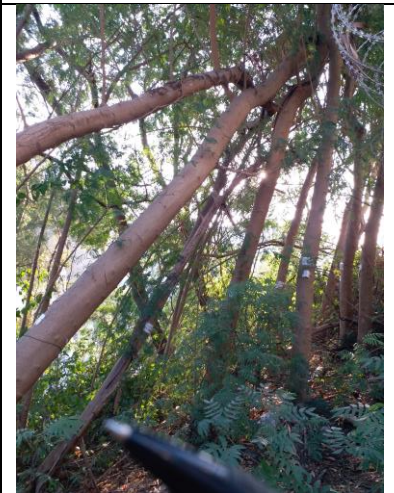
CT1902



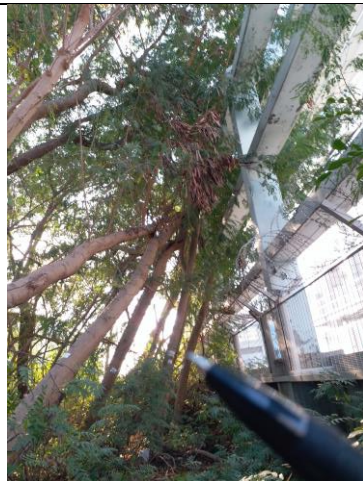
CT1904



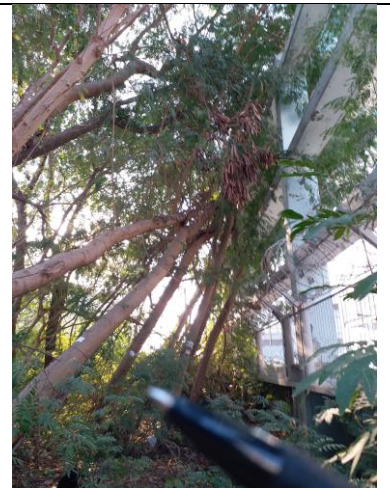
CT1905



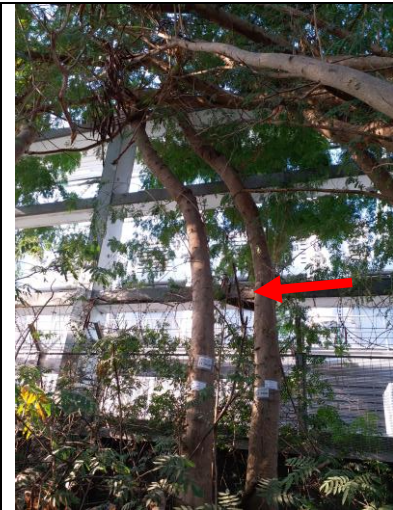
CT1906



CT1907



CT1908



CT1909



CT1910



CT1911



CT1912



CT1913



CT1914



CT1400



CT1384



CT1462



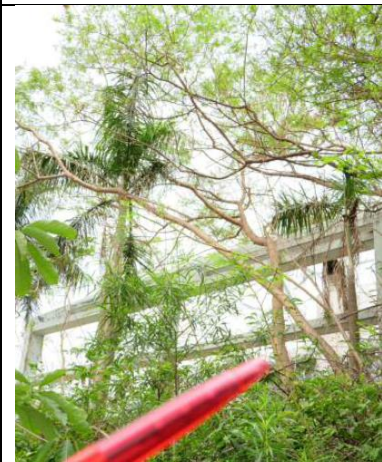
CT1893



CT1895



CT1917



CT1933



CT1934



CT1935



CT1373

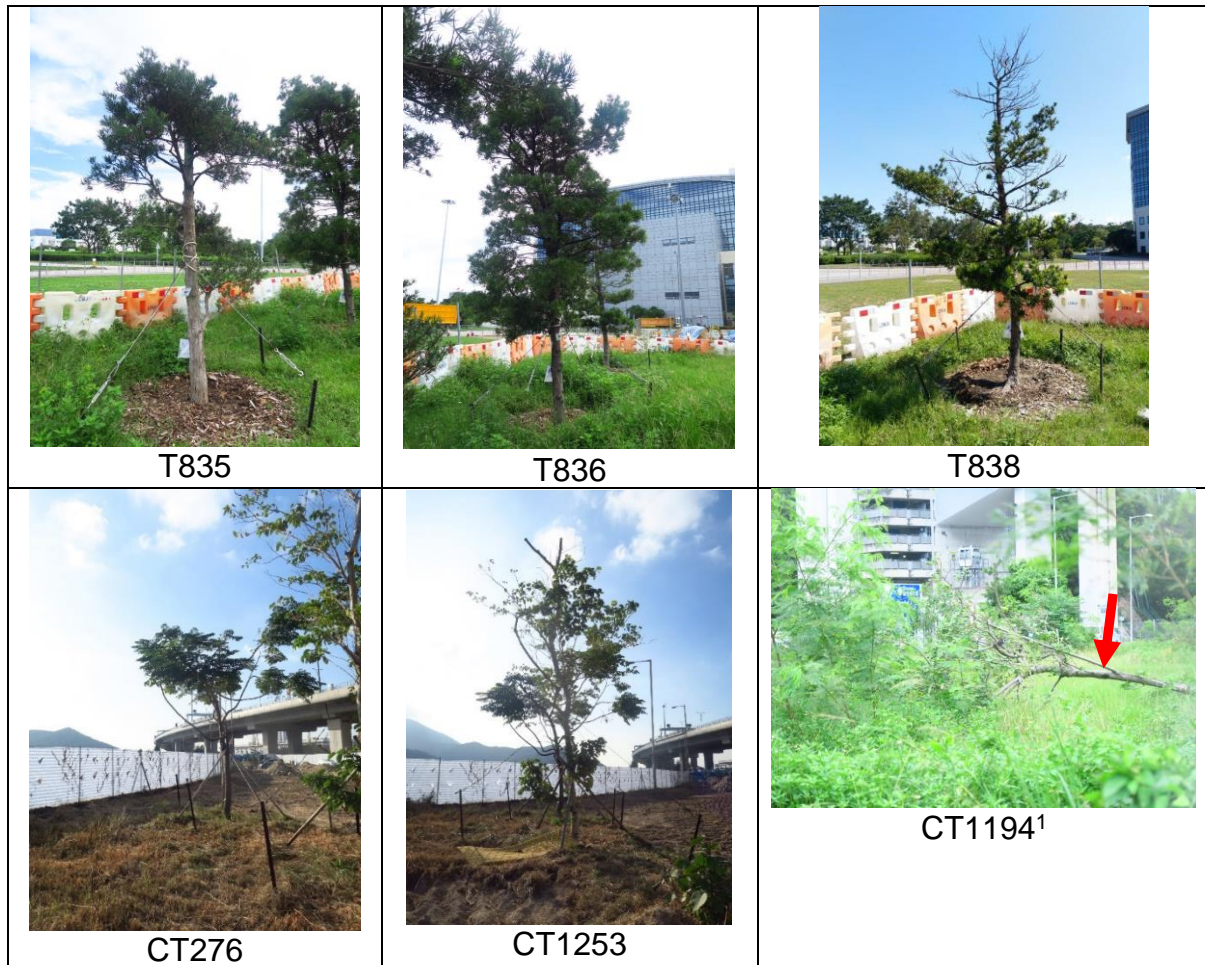


T0035



T0036

Photos of Transplanted Trees in the Reporting Period



Note:

1. Removed due to Typhoon Higos on 18 August 2020.

Photos of To-Be-Transplanted Trees in the Reporting Period



T812



T814



T815



T829



T830



T831